

Jul-Aug-Sep 2025



BRANCH LINE

NMRA'S FIRST REGION



BUILDING THE AGNEW

An O Scale Project by Phil Edholm

Story begins on page 25

The Branch Line
The Official Publication of the
Pacific Coast Region/ National
Model Railroad Association

Vol. 82, No. 3 3rd Qtr, 2025

The Branch Line is published quarterly to inform members of Region activities and to provide educational articles for the advancement of railroad history and the art and science of model railroading. It is distributed to members of the Pacific Coast Region. Electronic versions are posted on the website, www.pcrnmra.net.

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Inquiries regarding membership application, renewal, or change of address should be directed to the NMRA, P.O. Box 1328 Soddy Daisy, TN 37384-1328.

Branch Line
Deadlines for 2024/25

OCT-NOV-DEC 2025 issue

Articles due by
September 10, 2025

JAN-FEB-MAR 2026 issue

Articles due by
December 10, 2025

APR-MAY-JUN 2025 issue

Articles due by
March 10, 2026

JUL-AUG-SEP 2026 issue

Articles due by
June 10, 2026

Please direct questions to the
 PCR Publications Manager,
 Gus Campagna at
campgus@earthlink.net
 Or phone (707) 664-8466

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PCR Call Board

PCR Callboard

Updated April 22, 2025

Send any updates to webmaster@pcnmra.net



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PCR Call Board

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Continued on page 5

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**Chip Meriam,
PCR President**

Play Value - Or The Value Of Play

A term that has caught my attention in recent years is “Play Value.” Usually it appears in context of articles on operations: Time Table & Train Order, Track Warrants, Waybills, and Switch Lists. My personal favorite is the use of tokens, markers, or miniature figures to simulate the movement of trainmen while performing switching maneuvers (dancing). All of these operations-oriented ideas are deliberate complications designed to make it more fun (Play Value) to run model trains along a model railroad right-of-way. In fact, in the last issue of the **Branch Line**, Jon Schmidt had a fascinating article titled “Not Just Playing With Trains.”

But what is “play” anyway? Especially for those of us who are conditioned to the concept and virtue of work?

Here’s a clear definition:

A discussion by Peter Gray, Ph. D., [Research Professor, Boston College] in a November 2008 Psychology Today column, enumerates several points about the value of play:

- ♦ *Play is self-chosen and self-directed.*
- ♦ *Play is activity in which means are more valued than ends.*
- ♦ *Play has structure, or rules, which are not dictated by physical necessity but emanate from the minds of the players.*
- ♦ *Play is imaginative, non-literal, mentally removed in some way from “real” or “serious” life.*
- ♦ *Play involves an active, alert, but non-stressed frame of mind.*

Does this not describe our engagement with the hobby of model railroading? We choose this hobby on our own, and invest our time as we see fit. Generally, there are no “ends”. Our railroads are “never complete.”

As I observed above, play value is usually associated with operations. But, really, when you consider the seemingly unlimited facets of our hobby, there is play value in scratch-building, scenery creation, backdrop painting, electronics, mechanics....just about everything. I’ve even run across some folks who claim to “love” ballasting or track cleaning! To be honest, I derive a certain amount of gratification, or play value, from writing this column (the editor thinks I’m nuts).

Okay. We’ve agreed that model railroading is loaded with play value, but what about the value of that play? Is play just a whimsical waste of time? Is it a deterrent to our productivity? Should play be curtailed to create more time for serious stuff? Well, it turns out that play is beneficial to individuals and to society as a whole. If you’re in doubt about this, take a look at the following examples. I think you’ll find any guilt you might feel about investing your time with your hobby will be exonerated.

From HarborChase Senior Living:

When most people hear the word “play,” they likely picture children playing together, laughing, and enjoying themselves. However, very few imagine an older adult. For many, the idea of “playing” ends in childhood.

Whoever said that playtime was just for kids has yet to discover the marvelous benefits it brings to the lives of older adults!

Continued on page 8

From the President, continued

Jason Shron, founder and president of Rapido Trains in May 2025 Railroad Model Craftsman in a brief titled “Anxiety and Mental Health”:

In the same way one candle can extinguish the darkness from an entire room, our hobby has the ability to light us—even for just awhile—from our own personal darkness. For that reason alone, you should never feel like enjoying your hobby is a waste of time. Hobbies are designed to be a fun escape, to keep our minds occupied, and if they help you cope with everything happening in your world, all the better.

Sir Rod Stewart when asked in an interview with AARP Magazine, “What’s the difference between a good model railroad setup and a masterpiece?”

“There is no difference. It’s the pleasure it gives you. Everywhere I go, I take me paints and me tools, and I’ll build something instead of sitting around the hotel room all day. That’s what has kept me sane. I’m happiest when I’m in my workshop building something.”

From the National Institute For Play:

Sadly, too many of us have been trained from childhood to believe that play is “kid’s stuff” — not a productive use of time, something we leave behind when we join the work world.

Over the last 30 years, science has shown that play is very productive for humans at any age; we need play to keep our brains flexible, ward off depression, sustain optimism, and sharpen our social-emotional skills.

Making play a regular part of your life is incredibly powerful. Play supports our mental health, improves our ability to relate to others, and increases our drive and hope for the future. When play is missing or inadequate, it can have negative consequences in all of these areas. If you would like to be closer to the left side of the table below, try building more play into your daily or weekly routine for a while. Play won’t magically solve every problem, but it can — and will — give you a more optimistic outlook and mitigate stress levels during challenging life events.

When Life Is ...	Play-Filled	Play-Deprived
Trust	Life is experienced as a playground filled with chances to learn	Life is experienced as a proving ground — and often a battleground
Flexibility	Change brings exploration and new possibilities	Change creates fear and resistance
Optimism	Well-being and pleasure are expected	Discomfort and disappointment are expected
Problem-Solving	Problems are acknowledged and often foster skill development	Problems are hidden, denied, or avoided
Emotional Regulation	Stress is handled with resilience; the response is most often stability	Stress responses are often anger, rage, or withdrawal caused by low self-efficacy
Perseverance	Motivation is sustained from internal drive, mastery is sought	Motivation dissipates; equivocation, procrastination, and apathy arise
Empathy	Others’ feelings are recognized; support is often offered	Others’ feelings are not recognized; discord occurs
Openness	Life is vital; a strong sense of belonging fosters social cooperation	Life is dull; people become socially withdrawn, often with mild depression
Belonging	Behaviors are altruistic, leading to teamwork, community creation, and participation	Behaviors are callous, uncooperative, bullying, and self-centered

Now....go out and play!




Membership

Welcome Aboard ! PCR's Newest Members

Member	City, Sate	Date Joined
	Daylight	
Geoffrey Brown	Fresno CA	3/12/2025
Antonio Bernardo	Visalia CA	3/28/2025
	Coast	
David Batzloff	Alameda CA	3/28/2025
Gerald McKeegan	Walnut Creek CA	4/28/2025
Edward Sutorik	Berkeley CA	5/8/2025
Paul Perry	San Mateo CA	5/29/2025
	Sierra	
Harry Souders	Galt CA	3/28/2025
Dennis Leclert	Galt CA	3/28/2025
David Cladianos	Sacramento CA	3/28/2025
Tom Mcwhorter	Olivehurst CA	3/28/2025
Samantha Melton	Patterson CA	3/28/2025
Chuck Parnelli	Grass Valley CA	3/28/2025
Joshua Clements	Wheatland CA	4/1/2025
	Redwood Empire	
Richard Johnson	Vacaville CA	3/28/2025
Keith Flood	Santa Rosa CA	4/7/2025

PCR Membership Gauge

May 31, 2015—1,007
May 31, 2024—818
June 30, 2024—822
July 31, 2024—817
August 31, 2024—813
September 30, 2024—801
October 31, 2024—794
November 30, 2024—759
December 31, 2024—741
January 31, 2025—750
February 28, 2025—758
March 31, 2025—765
April 31, 2025—760
May 31, 2025—744

In Memoriam

<u>Member</u>	<u>City/State</u>	<u>Date Joined</u>
	Sierra	
John Krahn	Cameron Park CA	9/1/2011

PCR Membership Tracker

Division #Members

Coast	331
Sierra	223
Redwood Empire	104
Daylight	<u>86</u>
TOTAL	744

Have you changed your address or other membership information?

Please Notify Headquarters.

Email: nmrahq@nmra.org

Phone: 423-892-2846 (8:00 AM - 4:00 PM eastern)

LAST RUN

Ronald L. Plies

November 2, 1941 – May 31, 2025



Ron Plies, a past president of PCR from 2007 to 2011 and a former member of the Redwood Empire Division, passed away after a brief illness at his home in Boise, Idaho. He was surrounded by family members and dear friends as he left this earth.

Ron received his bachelor's degree from Pasadena Nazarene College and his master's degree from University of San Francisco, both of which prepared him for a professional career that spanned many different occupations. At various times, he worked as a probation officer, a tour bus driver, and an insurance agent/broker where he and his wife, Carol, were partners in the Ron Plies Insurance Agency in Fortuna. He was also a dedicated member of his church, and one of the positions that made him the happiest was when he served as a youth pastor at several churches in California and Washington.

Ron's lifelong passion for trains began with his father taking him to the train yard near their home in Inglewood, CA when Ron was a child. He was a longtime member of the Eel River Valley Model Railroad Club in Ferndale and served as its president. He was also an active member of the Southern Pacific Historical and Technical Society and attended many SPH&TS conventions.

Ron joined the NMRA in 1997 and immediately became involved, starting with being the Clinics chair for the 1999 PCR convention, followed by being elected Redwood Empire Division Director for the 2002 – 2006 term. He was chosen as The Redwood Empire Division's Member of the Year in 2007; and he was elected PCR President in 2007, serving until 2011. He was an enthusiastic contest entrant – he loved to talk about the models he was building – and won numerous awards at various events; and in 2008 he achieved one of his most cherished ambitions by becoming a Master Model railroader as MMR #409 (after which he would jokingly refer to himself as “Mr. Clean”).

After his PCR presidency was completed, he and Carol decided to retire and move to Idaho to be closer to family members (especially grandchildren). Once the move was complete, he joined the Caldwell (ID) Model Railroad Club and made new model railroad friends. He also continued to participate in SPH&TS events until his declining health curtailed his activities. Although he was progressively less able to be an active participant in the hobby, he was always happy to encourage other modelers in their efforts.

He is survived by his wife, Carol, his three children and their families (including 7 grandchildren), and numerous other family and friends.

Rest well, Ron. It is said that “Trains take us not just to places but to memories that last forever”; and the members of the PCR (and especially the RED) will treasure the memories you have left with us.



Have you joined the NMRA Interchange? It's the newest tool to help you connect with other members, gain knowledge and learn to modeling techniques. The Interchange is hosted on the Discord application which is available for Windows, Apple OS, Android and Apple devices. See instructions below for signing up.

Joining the NMRA Interchange

Speed Muller

IT Manager, NMRA

Have you joined the Interchange yet? If not, follow these steps to get connected!

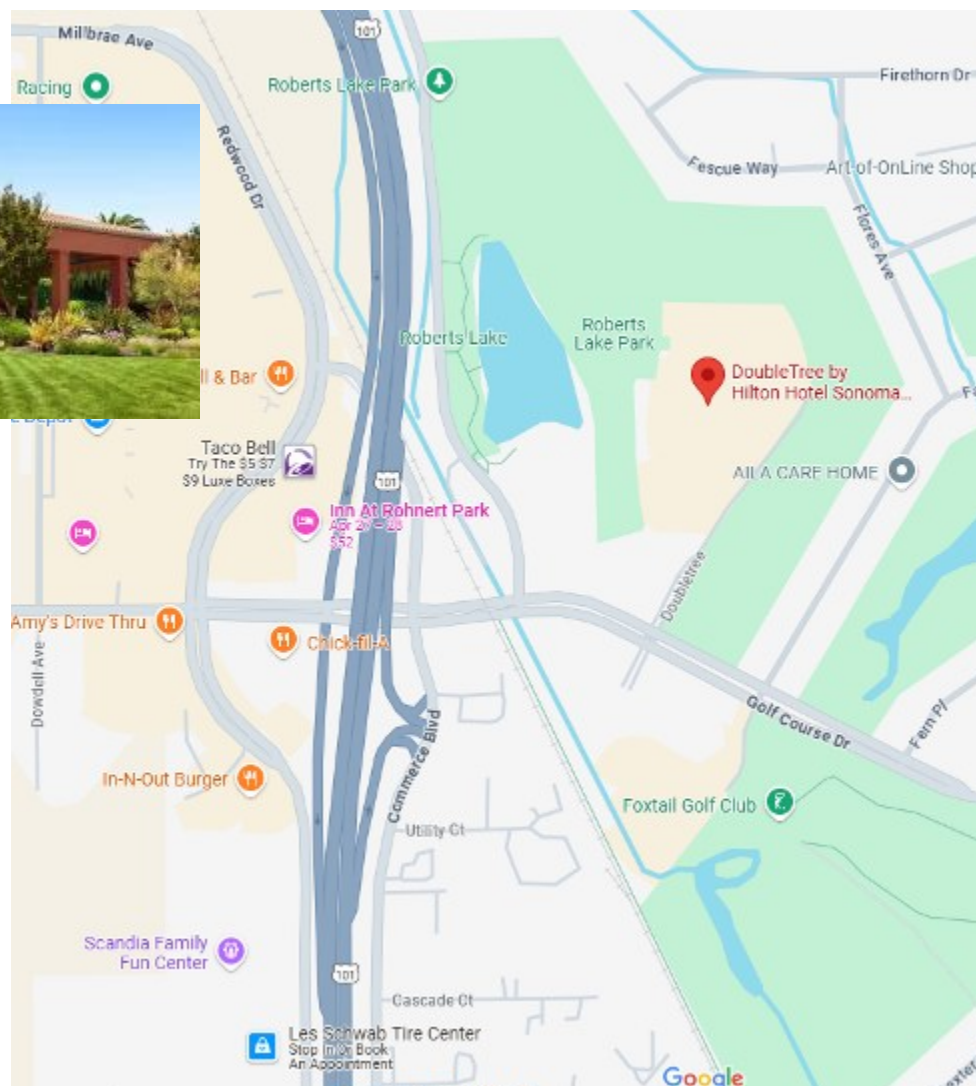
1. Go to nmra.org/nmra-interchange
2. It will ask you to log in to the NMRA website if you're not already logged in. If you are not registered with the NMRA website yet, now's the time to do it. Click the "Need a username? Register now" button, and get set up. Use the email address on file in the NMRA membership system (probably the one where you got this eBulletin). The system will email you a link to set your password.
3. Now you should be in the [NMRA Interchange](#) page. If not, click this link.
4. Click on [Member Info](#) in the top right corner and scroll down get your exact Member ID. You'll need that later.
5. Read through the page and then follow the "Interchange Server Invite" link at the bottom. This is a special link just for you and will look something like <https://discord.gg/xxxxxxx>
6. You will be taken to Discord (the platform that hosts the NMRA Interchange) where you will be prompted to sign in, if you already have an account there, or prompted to register, if not. If you need instructions for creating an account or logging into Discord they can be found [here](#).
7. Once you accept the NMRA Interchange Invite, and you are logged in, you will land in **#the-lobby** with a message on what to do next.
8. We suggest that you read and agree to the **#rules** first. Just click on **#rules** and then click on "thumbs up". Then click on **#the-lobby** again to do the most important step.
9. Discord and the NMRA need to check with each other, in order to verify that you are a current NMRA member with an email on file. You start the process with the **/onboard** command. Now, this is very sensitive to the type of device you are using, so just type the first few letters **/onb** at the bottom where you see a + sign in a circle. It will complete the command for you in a pop-up just above that. Click on the **/onboard** command. Add your membership number (6 characters followed by a space and then two more characters) just as you noted in step 4. Example: **/onboard input: 123456 0A**
10. Discord and the NMRA computers will have a chat, and then you will receive an email containing a **/clearance** command. That's your entry ticket. Copy and paste the whole line into **#the-lobby** (at the + sign again). Example: **/clearance input: 654321**
11. You're in! You will not have to do any of this again. If anything derails, you'll get a message telling you what to do next, and we also have Moderators in the Interchange standing by to help.
12. If all went well, you'll now see more than 350 channels (those # things). We suggest you start by going to **#introduce-yourself** and then scroll up to **#the-workbench** to see what others have done and shared! Then find your Region and Division and say "hello" there. And then start participating and enjoy!





EXTRA 2026 NORTH PCR CONVENTION APRIL 30 - MAY 3

- Full three-day program Thursday, Friday, Saturday
- Over 40 clinics
- Layout Tours
- Operating Sessions
- Contests
- Excursions
- Member Swap Meet
- FREE Companion Fare with Full Fare registration



INTERNATIONAL RAILFAIR



NOV 8-9 2025

OPERATING LAYOUTS! VENDOR TABLES!

ANDY FLETCHER LIVE DRAWING!

CHILDREN'S LAYOUTS & COLORING TABLE!

SQUARE FOOT DIORAMAS! DEMOS!

TRAINMART CONSIGNMENT STORE

\$15 SAT \$12 SUN at Gate \$12 SAT \$10 SUN Online

12 and under FREE! Cal Expo Parking: \$10

SATURDAY 10 AM - 5PM SUNDAY 10 AM - 4PM



CAL EXPO

BUILDING C

1600 EXPOSITION BLVD, SACRAMENTO

www.internationalrailfair.com





International Rail Fair is hosting a Square Foot Diorama Challenge. Build a diorama between now and November 8th, and display it during the show (on one of the NMRA tables). The rules are simple - the diorama has to fit on a 12" x 12" square base (or smaller) and there has to be an element that includes railroad track. The diorama can be in any scale, even a non standard 'railroad scale'. There will be a People's Choice award based on votes from our show's attendees. The International Rail Fair will be held at Cal Expo in Sacramento on November 8th and 9th 2025. The Challenge is a fun event that's aimed at giving our show attendees another thing to enjoy. I hope some of you are able to join in. I can't wait to see what you build.



Contest Room



By Paul Deis,
Contest Chair

Hello Fellow Model Railroaders or Railroad Modelers, which ever applies to you. I was just in my train room working on my railroad when the calendar on my phone chimed with a reminder. I need to submit my article for the *Branch Line*. It wasn't the first reminder. So why was I writing at the last minute? There are changes coming to the NMRA Contest Program and I have been trying to get you the most up to date information I can.

Why are things changing? A few reasons I can think of. One is technology, Laser cutting and 3D printing have caused a rethink of how a model is evaluated for conformance, construction and scratch-built. How about the photo contest? With focus stacking, Photo editing and AI there is much to be considered. Not all of this discussion is new. For example, when Fast Tracks first released their turnout jigs, some felt I disqualified a turnout built with the jig as being scratchbuilt. After much discussion it was determined the Fast Tracks Jig was just another tool.

Another change in the works is in relation to home layouts getting smaller. The old requirement in structures required a complete structure with all sides modeled. Many of us use a partial building either between the track and the backdrop, or between the track and an aisle. There are many wonderfully modeled structures built that way. Even some with fully modeled interior details. Now there is a new rule allowing a partial structure to be entered in the contest or evaluated for a merit award. Details of this change are coming soon from the NMRA National program chair.

I am sorry for the short column this time. So much is soon to be released from National, and I plan to include all in the next *Branch Line*.

Until next time, Keep-em on the rails

Paul



Achievement Program



By Earl Girbovan, MMR®
Achievement Manager

Congratulations to Phil Edholm for earning 4 AP awards:

- Dispatcher
- Volunteer
- Official
- Author

The awards were presented to Phil at the June 1st Coast Division meet. They reflect the time, energy and enthusiasm Phil has put into support of the hobby through his leadership in the Coast Division, being a member of both the Central Coast On30 and Pleasanton model railroad clubs and the time and energy put into the bi-monthly Saturday morning Zoom calls that keep the Coast Division unified.



MARK YOUR CALENDAR!



EXTRA 2026 NORTH PCR CONVENTION ROHNERT PARK, CA

Join us as we return to the DoubleTree Sonoma Wine Country Hotel in Rohnert Park April 30 - May 3, 2026 for the next annual PCR Convention. The official website is live at pccr2026.org, with even more details coming in July. Hotel rates are \$169 per night with free parking and no resort fees.

Get ready for an incredible lineup! We've already confirmed over 25 clinics, including 7 fun and interactive Make & Takes—with more clinics being added every week! You'll also enjoy at least 14 amazing layout tours (we're sure there will be more), featuring some new additions never before seen at PCR. And that's not all—7 unforgettable excursions are on the schedule, including fan-favorite Sturgeon's Mill, the new Russian River Rail Bikes, and a scenic day at the Western Railway Museum, complete with a picnic lunch and nostalgic train ride. You won't want to miss it!

This is shaping up to be one of our best conventions yet—see you there!

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EXTRA 2026 NORTH

**PCR CONVENTION
ROHNERT PARK, CA**

Registration will open July 3! And the fare options remain the same!

FULL FARE – \$160

Includes full convention registration, access to all clinics, contests, layout tours, and the Sunday Business Meeting/Awards Breakfast. Also includes a FREE Companion Fare, which grants the same access for one additional person.

EXPRESS FARE – \$110

Includes convention registration, clinics, contests, and layout tours for all three days. Does not include the Sunday breakfast.

DAY FARE – \$60 per day

Includes registration, clinics, contests, and layout tours for one specific day only—Thursday, Friday, or Saturday.

Clinicians Wanted!

Share your expertise with fellow modelers, rail history enthusiasts, and layout design and operations fans! Whether you've discovered a more efficient modeling technique, uncovered fascinating local railroad history, developed a hands-on skill to teach, or have a story from your layout that others can learn from—we want to hear from you.

The 2026 Convention is seeking knowledgeable and passionate presenters. We invite you to submit a clinic title along with a brief description of your presentation, hands-on workshop, or other session idea. Your insights can inspire and educate others in the hobby!

Visit our website at <https://pcr2026.org/clinics/> and fill out the clinic submission form to share your presentation with fellow attendees. Your participation is key to the success of the convention! For questions or help with the form, contact Jim Providenza at clinics@pcr2026.org.

Convention Merchandise Now Available



Order your convention merchandise today! Prices may increase later, so now's the perfect time to secure your items.

[Order HERE](#)



Pacific Coast Region SIG Report

by Seth Neumann



Seth Neumann,



Pacific Coast Region Special Interest Group (SIG) Report for 2nd Quarter 2025

We had several PCR Operators attend out of town meets including the Twin Cities, La Crosse WI, and Western Oregon Ops (WOOPS) in the Portland/Willamette Valley this quarter. We are expecting a great operating program at NMRA's annual convention "STATION No. VI", in Novi, MI July 14-19 (note it starts on Monday – NOT Sunday) and the major SIGs are planning

to put on their usual programs of Design, Operation, Electronics, and "Birds of a Feather" groups. <https://nmra2025.com/registration/>

Both Layout design (LDSIG) and Operations (OPSIG) have been publishing their magazines regularly. OPSIG is working on a new skills development program to help new operators hone their skills and to be able to demonstrate proficiency to hosts.

As NMRA SIG Program Coordinator I'm here to help you form a SIG around any topic or grouping of individuals. So if you want to bring people together around a prototype, a technology a historical period or a group of people who have not historically participated in in the NMRA, reach out to me at sigs@nmra.org and I'll help set you up as a SIG.

Cydney George-Abatecola has posted a "Women in Model Railroading SIG" report elsewhere in the Branch-line.

As always if you are interested in these programs and don't know where to start (especially operations) please contact me at sneumann@pacbell.net

Seth Neumann



The Answers Are Out There

By Robert Pethoud



The goal of my portable one-turnout layout is a convincing industrial scene on which to demonstrate how much enjoyable and realistic operation is possible with the simplest possible track plan. The small cannery structure we finished last time will accept a maximum of two freight cars; this time we'll begin scratch-building a much larger structure that can serve up to four standard 40-foot freight cars at once.

To contrast with the recently completed brick building, I wanted this one to be made of wood. Board and batten siding from Northeastern Scale Lumber is what I chose, with battens 3/32" apart on 1/16" thick basswood sold in pieces measuring 3" by 24". One-sixteenth inch thick basswood is not rigid enough for a cannery this large, so I planned to glue this siding to a shell of 1/8" MDF (medium density fiberboard) reinforced with lengths of square poplar bracing at the corners. Since I was unable to find any of the MDF in my vicinity, I decided to construct the shell from some 1/8" basswood I had in stock.

I began by cutting two pieces of the 1/8" basswood for the trackside walls, a long narrow one with three freight car spots for loads out (see photo 1) and a taller one with a gable and two doors for inbound loads (see photo 2). 18 Grandt Line windows (#5031 36"x 64") and four Grandt Line freight doors (#5080 D&RGW Station Baggage Doors) were chosen from my stash. The right hand door on the taller piece is larger than the others in order to accommodate a crane, which explains its odd shape. I used a Chooch door for this (#7116 Wood Braced Factory Door).

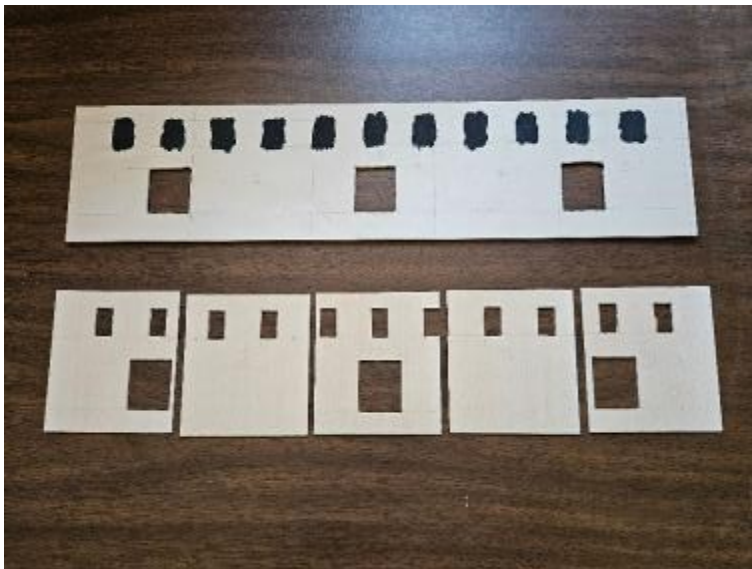


Photo 1



Photo 2

Continued on page 20

Answers, continued

After careful measuring, I drew locations for all the doors and windows on the shell pieces. Then I cut openings for the doors, all of which are recessed and a few may be open, but I just painted the window areas black, since visitors will not be able to see inside.

The gabled wall will stand about two scale feet from a freight car on the spur, but the long wall—which will fill the area between the gabled wall and our previously built brick structure—will be set back six or eight more feet to allow for a loading dock.

The board and batten wall coverings were cut $\frac{1}{4}$ " shorter than the shell pieces to which they will be glued to allow a concrete foundation to be visible below the B & B. To depict this foundation, I used a brush to paint a coat of CraftSmart Suede acrylic craft paint on the bottom $\frac{1}{4}$ " of each shell piece.

After cutting rectangular holes for both the doors and windows in the $\frac{1}{16}$ " B & B pieces, I stained them to represent weathered wood. I brushed on two coats each of black stain (1 tsp of India ink in a pint of 70% isopropyl alcohol) and brown stain (1 tsp of brown leather dye in a pint of alcohol). See photo 3 to appreciate the difference in appearance the stain makes.

If you plan to have a newly painted or well-maintained structure with an opaque color coat on the siding, then you could skip the stain. I wanted some weathered wood to show through a worn and peeling coat of paint, so I brushed on a wash of DecoArt Americana Sage Mint. From experience, I've found that it takes two or three coats of these cheap acrylic craft paints to end up with a newly painted appearance. When my first coat hid too much of the stain, I immediately added water to make it a wash. Check out photo 4 to see whether it worked.

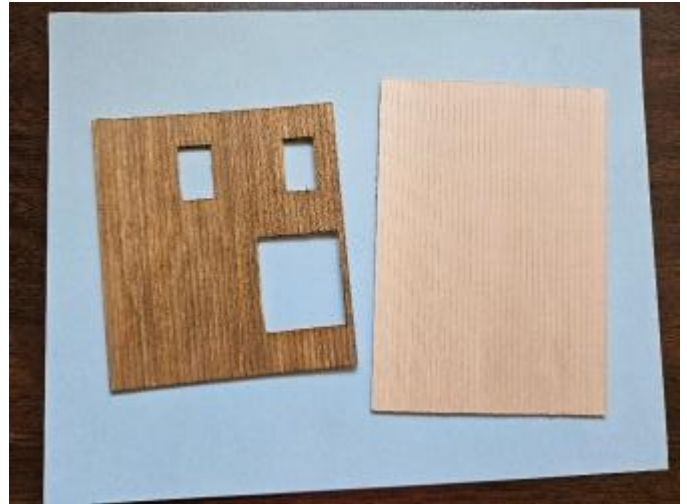


Photo 3



Photo 4

This portion of the cannery will be weathered green with brown trim. Painting the windows and doors was a job for rattle cans. I used masking tape to attach the castings to a length of 1x2 scrap wood and sprayed them with Rust-Oleum Painter's Touch Satin Heirloom White paint + primer. After this had dried for a few days in the Fresno heat, I used an X-acto #11 blade and a sheet of glass to cut masking tape rectangles to cover the windowpanes and muntins. Then I sprayed all the windows again with the Heirloom White to keep the brown trim paint from sneaking under the tape. Finally, after waiting a couple more days, I sprayed all the castings with brown in the form of Rust-Oleum gloss Chestnut.

Next time, we'll assemble this structure and add signage, a loading dock, a crane, and—oh, yes—a roof.

Until next time, reach me with comments and/or questions at pethoud@comcast.net



NATIONAL MODEL RAILROAD ASSOCIATION
2025 National Convention

STATION No. VI

Hosted by:
the NORTH CENTRAL REGION

July 14-19, 2025

Open to ALL model railroaders! You do not have to be an NMRA member to attend! **REGISTRATION IS OPEN!!**

Our location – Sheraton Detroit-Nov, MI
A suburban location with FREE parking!



CONVENTION GOALS and HIGHLIGHTS -

Our main objectives are to provide events of value, items attendees will see as worth their money, all at the lowest possible cost. ** We will honor the traditions of past conventions but at lower cost and with greater flexibility. ** This convention will be shorter, starting at 1 PM on Monday and continuing through Saturday afternoon with a banquet Saturday evening. ** Note that registration includes all layout visits and operating sessions, and all activities at the hotel except Modeling with the Masters, and admittance to the National Train Show. ** We will support all the traditional activities of the SIG's. ** The National Contest will be fully supported. The contest will be in a prominent room and will be run by the National Contest team. There will be space for model displays, modular layouts and other functions. Those putting their models on display will have the option of entering the People's Choice Contest. ** We will have several prototype tours which may include a Ford plant tour and a cruise on the Detroit River with an emphasis on the industrial aspects of the waterfront. ** We expect to have at least 90 clinics. The clinics will be varied, and each will be given twice. ** We expect to have at least 60 layouts open for visit. Most layouts will be at least 900 sq. ft. in size running to over 3,000 sq. ft. in one or more cases. All layout visits and op sessions will be self-guided so you may take your time.

<https://nmra2025.com/>

Redding Model Train Show

August 9th & 10th

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Saturday Show Hours: 10AM-7PM

Sunday Show Hours: 10AM-4PM

Ages 13 & up: \$7

Ages 5-12 (with adult): \$5

Ages 4 & under (with adult): Free



Questions? reddingtrainshow@gmail.com

Tales of the Santa Cruz Northern

By: Jim Provenza

A Tale of Five or Six Boxes



Hmmm, so there are these four boxes sitting on the table at the Cabin – our family place in the Santa Cruz Mountains. They are “extras” in case I somehow complete work on the current project – or find that I am out of a needed part.

Well, actually there are 5 boxes with car kits on the table. Counting the one I have open there are two Walthers, one Intermountain and... one Ulrich and a Silverstreak. Oh, and a ‘Yellow Box’ Athearn. But it doesn’t have a car kit in it. Lots of car parts though...



A pile of boxes at the cabin

I’ve mentioned before that I enjoy working on freight cars. Building kits, modifying or adding details to already built cars, kit-bashing, occasionally scratch-building. Almost always in styrene, but with the occasional foray into wood, resin or metal. I get a particular joy out of resurrecting a derelict. In fact, the car that I am currently working on is just such a case – a Walthers tank car that had ‘deconstructed itself’ and was missing some parts. But the story of that car will await another day.



The Silverstreak box and its boxcar – In need of some TLC and replacement parts.

Let’s start with the Silverstreak kit. The box is proudly labeled on the side, “Manufactured by Wm. K. Walthers, Inc. Milwaukee, WI 53216”. There is no car information on the box ends. Inside is a model of a 40’ wood side double sheathed boxcar, with the sides painted Vermillion Red and lettered for the Great Northern. It is a ‘standard’ wood car kit, typical of car kits before the injected molded styrene revolution. A milled roof is glued to two blocks of wood to support the ends and they, in turn, are glued to a wood floor. Cast metal ends, fish belly center sill, and body bolsters are attached to the basic frame. Formed metal roof ribs are bent to the shape of the roof and the ends are crimped over the roof edges. The 3 ply plywood sides are glued in place and details, trucks and couplers added (though missing at this point in time).

The two-tone green Ulrich box is labeled for ‘GS Gondola Kit, No 113 C&S’ on one end. Stamped above this information is ‘\$2.90’. Ah, inflation is an insidious thing. The

economists all say it is better than deflation of the currency, but you have to wonder...

The instructions are still in the kit box and show a date of 6-52. Hmmm, \$3 in 1952 is worth about \$36 today. Makes an Accurail kit seem very reasonable and other manufacturers’ products not far out of the ball park!

So, the car in the box is around 70 years old! All cast metal, and the metal itself is in good condition! Those folks at Ulrich knew what they were doing. The car had been assembled well; the drop doors on one side had all worked loose but are accounted for. No couplers, but that is okay. Trucks attached are sprung metal with relatively free rolling metal wheel sets. You can see the lacquer used to insulate the metal wheels on one side as a shiny coat of clear paint on the axles.



The Ulrich GS Gon. A wonderful model, still in very good shape. It will get some work to upgrade lettering.

Continued on page 24

Tales of the SCN, continued



The Walther's flat car, still all wrapped up.

The Walther's box is maybe 10-15 years old. A simple 42' flat car kit with everything including the instructions still sealed in a poly bag. Almost 'shake the box'! Plastic wheels on brass axles, black engineering plastic one-piece trucks. Pad printed lettering is much sharper than what you got in a car kit several decades earlier. A good example of what I would term a second generation injected molded styrene kit – even if it still came with horn hook couplers.

The red and white InterMountain box has a special label on one end: 'KC101X – SP/T&NO B-50-25 12 Panel 10'0" IH Postwar AAR Box Car kit, includes conversion parts and decals SPEEDWITCH MEDIA'. So here we have what might be called a high end second generation plastic kit, which in turn is being sold with upgraded conversion parts to make a really accurate model of a specific prototype car. Looking through the various instruction sheets you find out that the basic InterMountain kit is an accurate model of specific Great Northern and Spokane, Portland and Seattle groups of cars. Full stop. The Speedwitch Media conversion includes resin detail parts for the ends, roof and doors – as well as very detailed instructions to model the SP B-50-25 cars in several levels of detail.



The InterMountain / Speedwitch Media kit.

What about that Athearn 'yellow box'? As well as the Athearn 'blue box' shake the box kits there was, in fact, a series of Athearn kits sold in yellow boxes. I believe they were some of the early injection molded styrene models. The contents of this yellow box?

The printed end label originally read: '2206-1:98 PRR Box Car Ready-To-Roll'; a faded sticker has a price of \$1.25. The end could be an archeological dig all on its own. It has been written over at least twice. The box itself contains my supply of most used car detail parts, everything from locomotive

airhorns to Westinghouse AB brake systems. It includes parts from A-Line, Builders in Scale, CalScale, Detail Associates, Details West, MV Products, Tichy, Walther's, and Westerfield among others. I am sure in the future it will include 3D printed parts from new manufacturers.

It is fascinating to be able to work on models spanning over 70 years. Looking at the kits side by side gives one a sense of the developments in technology over the decades. Hobbyists turned manufacturers have worked with these developments to give us better, more detailed models that in many cases are also prototypically accurate. This is, of course, a really good thing.

And yet... it is fun to take some poor, old, worn out car kit car, bring it up to today's standards, and put it back into service.

And I suspect that the folks who created the Ulrich and Silverstreak kits would be amazed to find them still in service 7 decades after they were produced!



Building the Agnew/Pajaro Station in O Scale – Part 1

By Phil Edholm

As part of the layout I am building using my On30 modules in a fixed configuration, I needed a station for the town of Pajaro. Pajaro is between Watsonville and Moss Landing, California on my railroad—as it was on the prototype Pajaro Valley Consolidated Railroad. This area was built in the late 1800s for sugar beets with a major beet processing plant in Watsonville. These are some photos of the completed model which qualified for a merit award and won 3rd place at the 2025 PCR Convention.



The original Pajaro station followed a design that was used on many stations on the PVCRR and on the adjacent South Pacific Coast Railroad that ran from Alameda to Santa Cruz, just north of Watsonville. This is a photo of the Pajaro Depot circa 1900. As I have visited most of the current depots in the central California area, I knew the Agnew Depot in Santa Clara was similar design. And while the Pajaro depot is long gone, the Agnew depot is still standing and used by the California Central Railroad Society for their HO scale layout.

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The Agnew Depot was built in 1877 on property donated by Abram Agnew. It was initially called the Agnew's depot, but that name was later shortened to just Agnew. It was a station on the South Pacific Coast Railroad on the narrow-gauge line from Alameda to Santa Cruz. For many years, the station was the primary access to the Agnews State Hospital, originally known as the "Great Asylum for the Insane". The station was transferred to the Southern Pacific in 1887 and the SPCRR was later absorbed by the SP. The line was converted to standard gauge in the early 1900s. After rail passenger service to the station was abandoned, the structure was purchased by the California Central Model Railroad Club in 1963. The current structure is a Historic Landmark in Santa Clara, CA. The photo shows the station in 2023, painted in SPCRR colors. In 2024, the station was re-painted in Southern Pacific colors.



I decided to build a model based on the Agnew depot with as much adherence to the prototype as possible. To begin the modeling journey, I gathered some additional photos of the current structure from the Internet.

While the structure enjoyed 70 plus years as a station, since 1963 it has been the location of an operating model railroad, the CCMRC. They have built a great model of western railroading in California with the line from the San Francisco



Bay area up to Oregon. The club has regular operating sessions and public events. However, the word in the club is the depot is now almost totally supported by the benchwork of the model railroad!

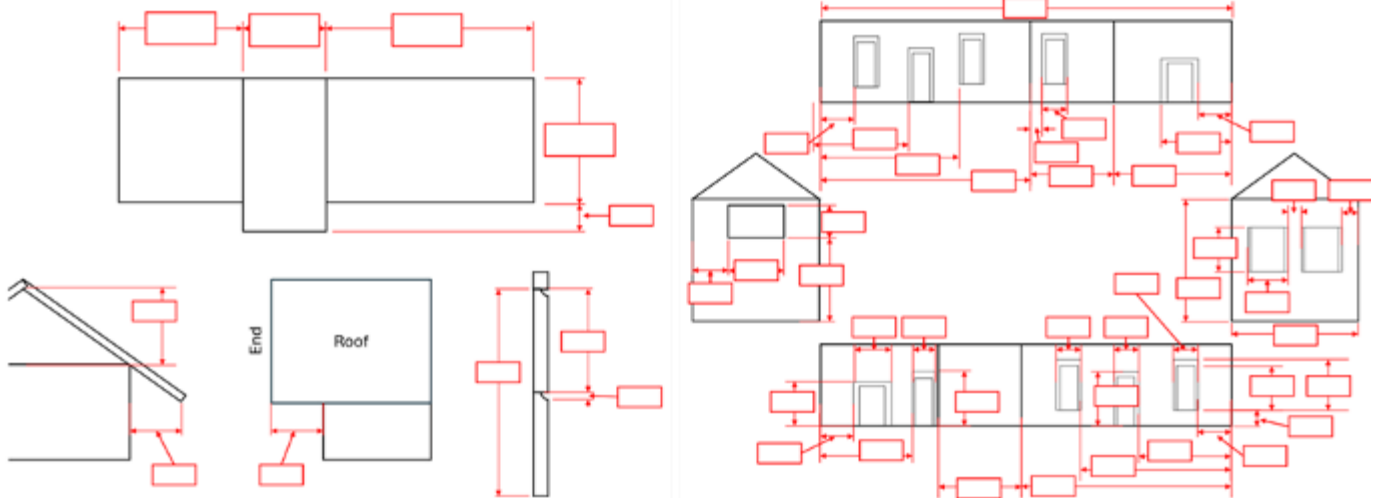
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I was also able to get some photos of the depot from the past. These came from the archives at the Society for the Preservation of Carter Brothers resources (SPCRR). They have an extensive library of early narrow-gauge railroading and are preserving many of Carter Brothers railroad cars. The Carter Brothers built many narrow-gauge railroads and cars in California and the West including the South Pacific Coast RR.



These two photos show the Agnew depot at two different times. The photo on the left was taken soon after the depot was constructed, while the photo on the right is later. Note the addition of the extension of the office under the roof and the name change from Agnew's to just Agnew. The photo on the right reflects the era of Southern Pacific control as it includes distance markers to locations on the SP.

Using the photos from the internet, I did rough measurements using PowerPoint as a drawing tool. This allowed me to make up a rough drawing with blanks to fill out for measurements of the actual structure. As the depot is only about 30



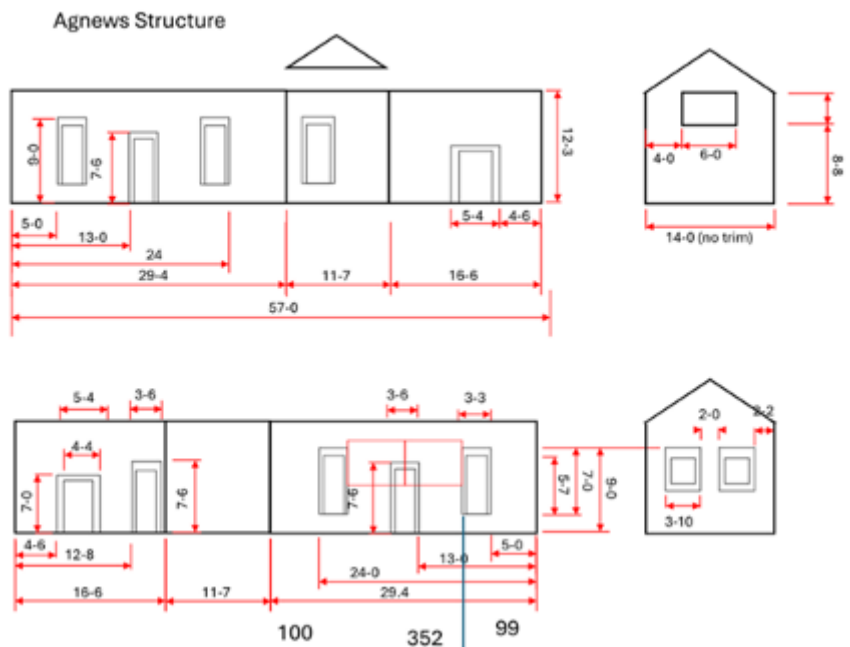
miles from my house, I scheduled a visit to measure the building. On the visit I used the measuring tool on my iPhone to make quick measurements of the structure. This tool allows you to locate one end of the measurement and click to locate. Then you move the phone and the display shows the accurate distance from where you clicked to where the phone is. You can then read the measurement. I found this made measuring the distance from the end of a wall to different doors and windows to be much easier than using a tape (which I brought but did not use).

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The result was a revised drawing that included the measurements from the actual building. I was able to get almost all the measurements I needed from this, but I missed a couple of measurements: the roof overhangs and the roof angle. I was able to determine those by using photos and PowerPoint.

This exercise illuminated one issue in my overall model. I had already cut the base for the structure to fit the available area on my layout. I normally use 3/16" Masonite or plywood as a base for my structures. I wrap the base in plastic wrap and use Sculptamold® up to the base for the landscaping. The depot is intended to go inside the curve and the overall length needed to be about 12 inches as a road must fit to the

left between the depot and a team track ramp. Because of this, the base was cut to 12 inches, so the structure could only be 47 scale feet long, versus the 57 feet of the actual structure.



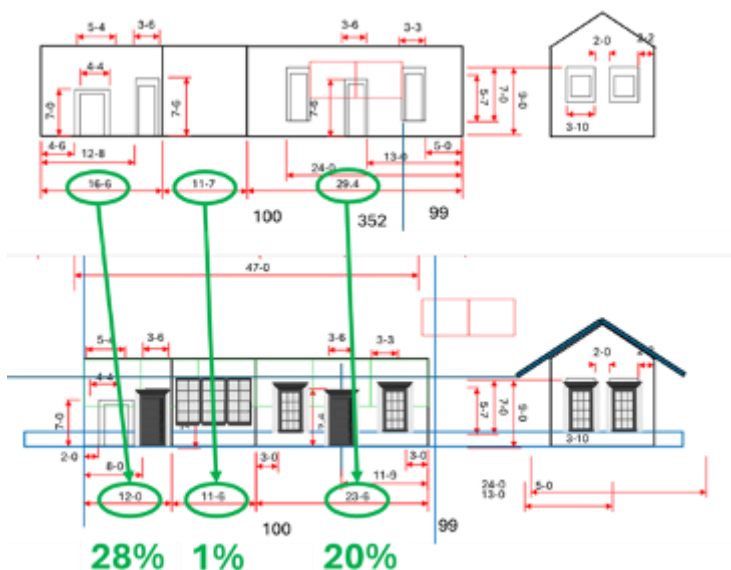
Layout Location



I realized I would need to compress the structure to fit in the space. I made the decision to only compress the overall length, not the width, height, or the other dimensions. As I intended to make visible interiors for the office and waiting room and to keep the baggage area closed, I decided to reduce that the most. The drawing shows how the overall length was compressed.

The office was left relatively intact as that affected the interior space available and the roof line for the dormer. The waiting room was reduced by about 20% while the baggage room was reduced by 28%. In the end this gave an overall length of 47 feet. The window and door spacing was made to reflect the original spacing with the new dimensions. I now had the overall dimensions of the build.

an



Continued on page 29

In addition to measuring, I took a number of photos of the structure. The structure was repainted into Southern Pacific depot colors in 2024, so they do not reflect the SPCRR colors I decided to use for my structure. In my model of the central California Coast, the SP had not yet taken over the narrow-gauge railroads.

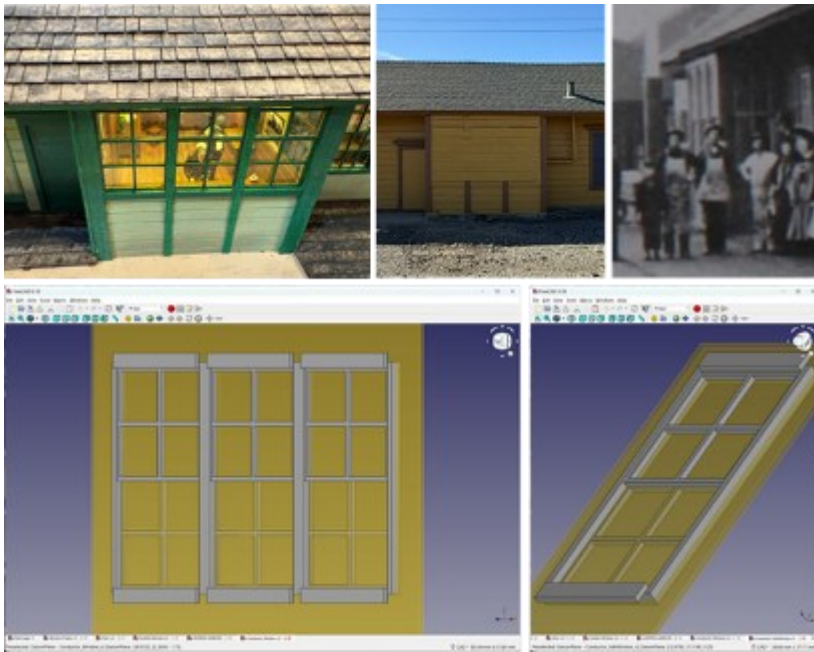
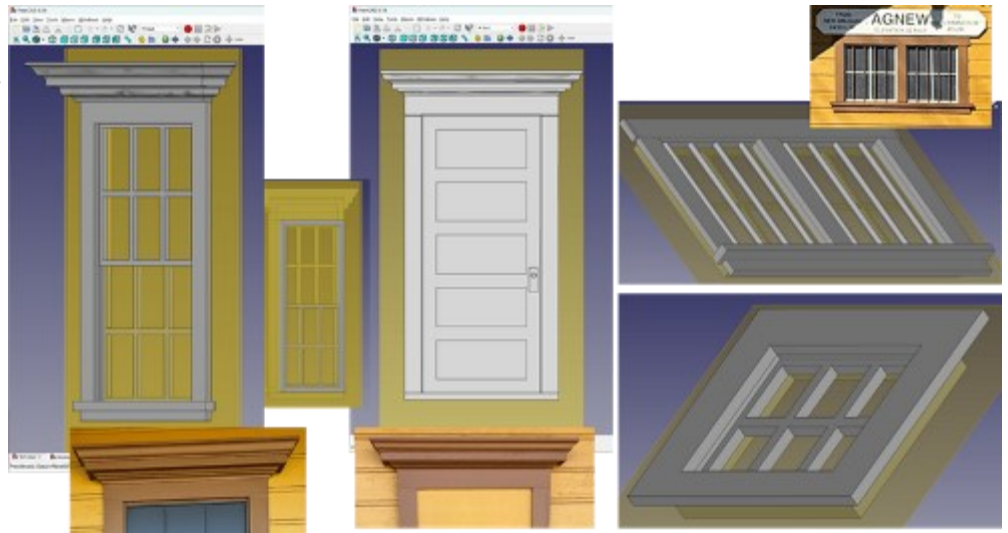


I also took several photos of the unique architectural details.



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One thing that became very clear through this process was the unique trims on the doors and windows are not readily available as commercial castings. Therefore I decided to design them in CAD and 3D print in resin. I use FreeCAD for design and Fran Foley of Foley Locomotive and Car shops agreed to print my items. These are the designs for the windows and doors. The details on the top of the doors and windows are much like fireplace mantels. They were built by padding a square block and then removing the material for the final shape. The ends can be done with a single drawing using the mirror function.



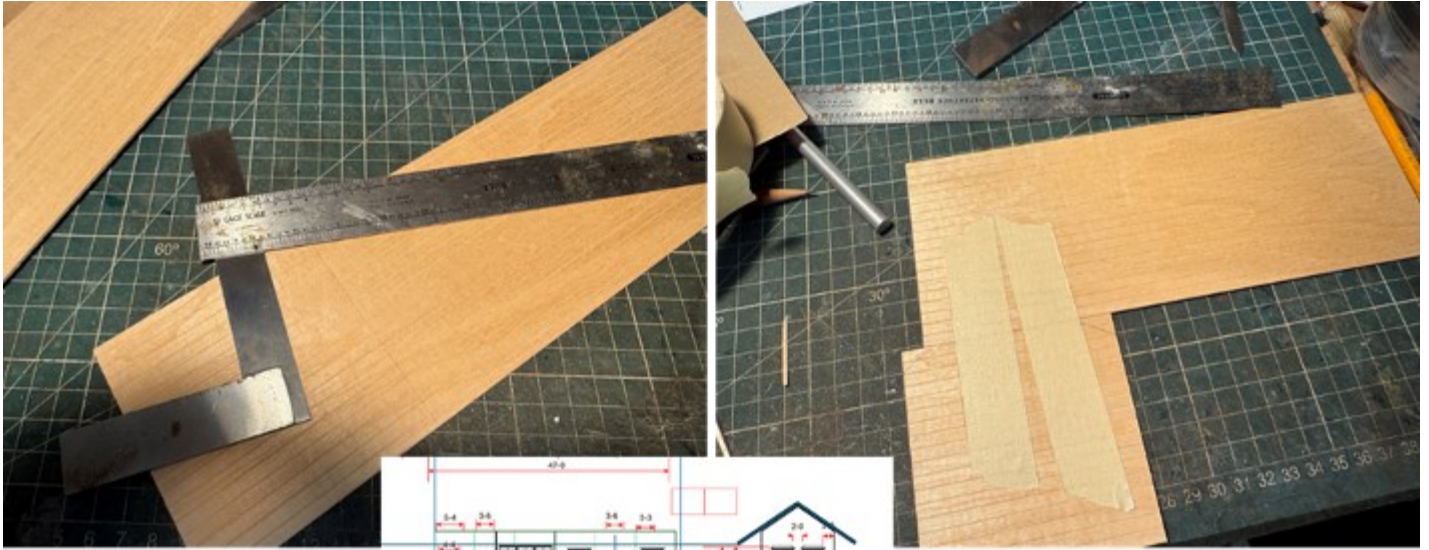
The windows for the office were more complicated as the vertical trim wood needed to run through the window and up the structure. The trim below the window can be seen in the center prototype photo. The model photo shows how this turned out.

The details were also recreated using FreeCAD and resin printing.



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With the detail parts designed and off for printing, it was time to lay out and cut the walls, choose exterior paints, and finish the exterior walls.



Based on the building design, cutting the wall material is the next step. The walls are 1/16" thick scribed siding from Northeastern Scale Lumber with a spacing of 3/16". IN O scale, 3/16" comes out to a spacing of 9 inches, very close to the 9.5 inches on the prototype. While the width of the scribing is slightly less than the gaps in the prototype boards, it would be hard to measure.



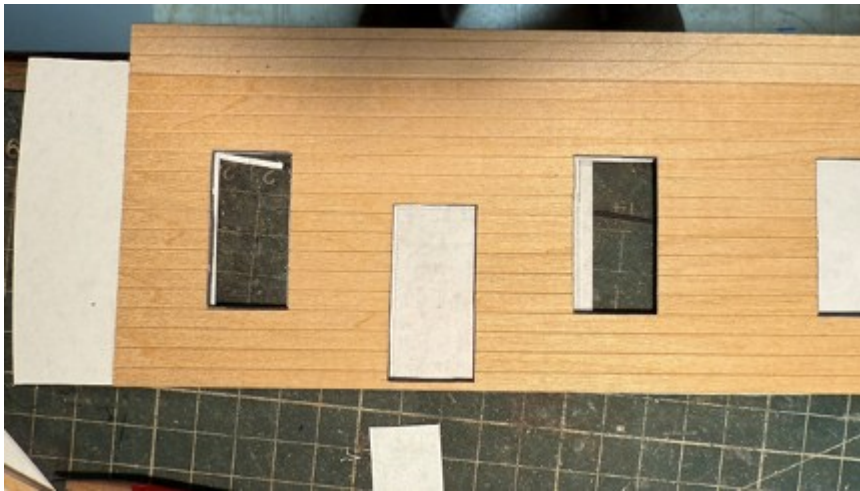
The lines were laid out directly onto the surface of the siding as I find trying to do it on the back in reverse is too complicated. Any mistake lines can be easily sanded out using 400 sandpaper. Before cutting the lines, the siding was slightly grained using 100 sandpaper and scraping across the siding in the grain direction. The lines were laid out based on the design with a scale and a square. The square is one that the large part is thinner, so it fits well on top of the siding. As the end walls were taller than the 3" of the siding pieces, I used masking tape on both sides to make the larger piece and have the cuts be accurate. Take care to align the gaps. The cuts were primarily done using a scalpel, but some were done with an Xacto blade. The scalpels can be purchased from a tool vendor at a Train Show. For cuts, the cutting straightedges (6" and 12") from MicroMark were used. These are great tools to keep from slicing fingers.

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In addition to cutting the primary walls and windows/doors, the small dormer was cut out. In the photo you can see the small wings that were cut to match the roof angle and make putting the dormer to the roof easier. Also, note the installed window is painted brown on the inside. This was done to all windows so when they were installed with the printed interiors they would match.

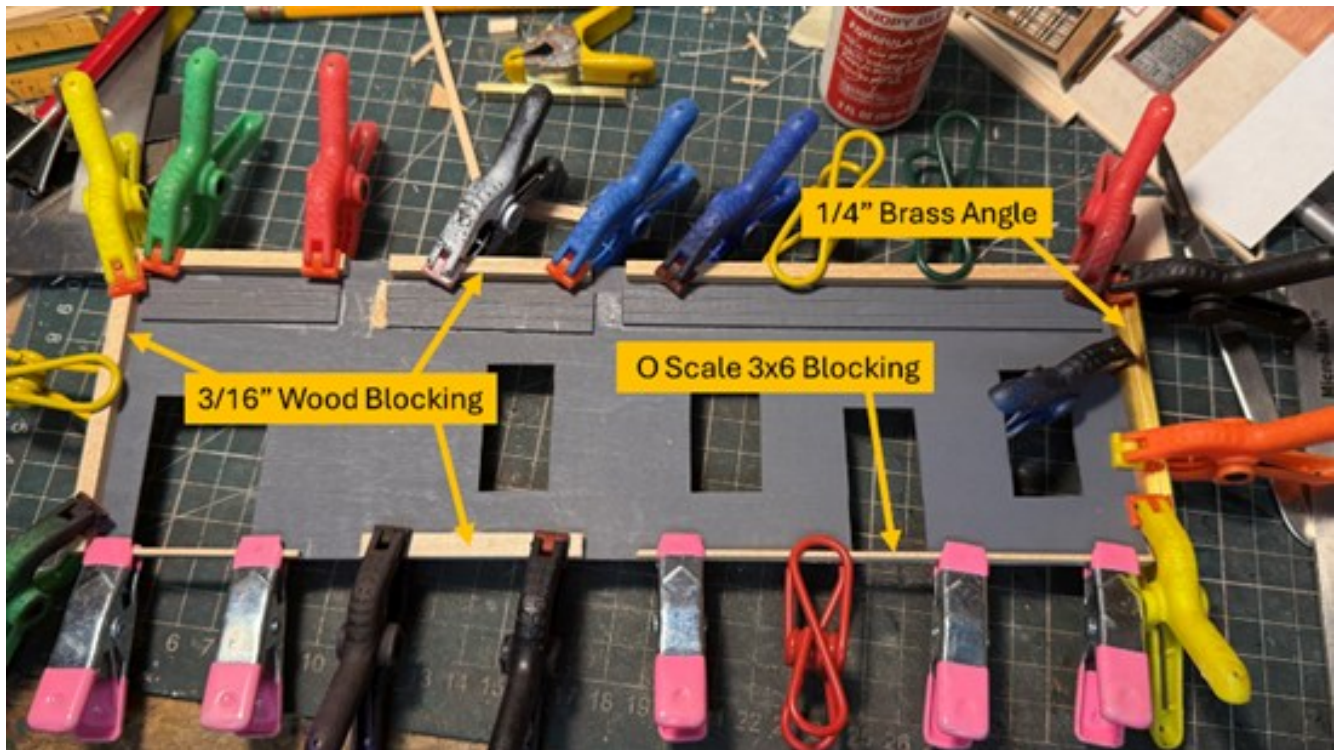
While the buildup of the printed interiors will be covered below, the window cut out position was done at this time when the walls were flat. This allows the cut-out in the printed interior to match the cuts in the wood wall.



After getting all the wall cutouts done, the interior was painted with Rustoleum grey primer to reduce any warping when the exterior was painted. Then the wall bracing was installed. For the walls where the printed interiors were to join, a 1/4" brass angle was used as a brace so it would not interfere with printed interior. These and other pieces were glued together using Canopy glue.



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This photo is the long “back” wall with the braces installed. In the freight room, which I decided would not have a visible interior, wood was used for wall blocking. A 1/4”x1/4” piece was added on the top of the wall, both to prevent warping and to contain any light. At the bottom of the wall, an O scale 3x6 was added. This was done to raise the floor up slightly to match the prototype and to have a good glued surface to mount the wall to the base. A range of small clamps was used. All of the wall bracing was completed at this step.



Once the walls were to this point, it was time to begin the process of choosing paints/colors. I tried rattle can sprays, craft paints, and Tru-Color® options. I had decided to do South Pacific Coast colors in lieu of the Southern Pacific Yellow/brown. Generally, spray paints are better for the parts, while craft paints are good for the scribed wood. After testing different paints, I decided on Rustoleum® Meadow Green for the trim pieces and Folk Art® Wild Wasabi (9455) craft paint for the walls.

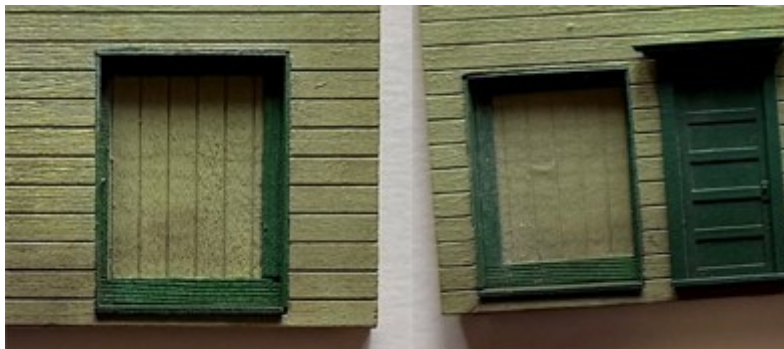
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The resin-printed detail parts were painted with the rattle can spray paint (primer and color) on masking boards. This was also done for the trim pieces. Before painting, the trim pieces were grained using a razor saw. Even in O scale this is too large of a grain, but it makes the models look more realistic in my mind. After spraying the color, all of the trim was rubbed with white chalk to lighten the color to show a level of fading. After this step all the parts were sprayed with a coat of matte clear enamel spray paint.

The walls were painted with the

craft paint by using undiluted paint lightly on the brush and rubbing onto the walls.



The walls were completed flat on the workbench before final assembly. I find this is the best way to manage detail for both interior and exterior. The first element of the wall assembly was the freight doors. The basic door is the same scribed siding as the walls with vertical placement and trim added to frame the hanging door. Trim was added around the door openings to frame the door and the door was glued from the inside (back). When cutting trim that is pre-painted, the ends are not painted. When using a rattle can for color, spraying a spot on a scrap piece of paper lets you dip the ends and color them.

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Next the windows were added to the walls. At the same time, holes were drilled in the walls for the pins on the eave braces. These were all drilled at the same height. A brass wire was used to secure the braces into the wall and the hole in the 3D casting.

Once the windows and eave brace pins were installed, the walls were weathered. This was done with chalks. I use a glass backing loaf pan (8"x4"x3") and a micro grater from the dollar store to grate chalks. White chalk was applied first starting from the bottom and working up with less chalk higher on the wall. This creates a fading effect that simulates the effect of the sun down the walls. The south facing wall (baggage room) was made slightly lighter as well. Next black and brown chalks were lightly added at the bottom of each wall to simulate dirt that get on the walls from the ground. This was heavier on the back side and ends where there is not a deck in the final model. After the chalk was complete the walls were sprayed with matte clear.



The walls were assembled by adding the eave braces, signs, and glazing. At this point the printed interiors were added. Creating these will be covered in the next article.

This is an example of the interior wall photo images glued to a wall. This is the interior Waiting Room wall adjacent to the office. The cut-out will become a ticket window between the Waiting Room and the Office. Note that the left edge of the photo is not glued to the wall so the brass corner block can slide under it during final assembly.

Next month we will cover making the interior walls and signs, building assembly, interior details, roof structure shales and weathering, and the outside decks.





Looking Back

Retrospective Memories of Ron Plies, MMR

First, let me preface this by saying that on my “short list” of people I would have wanted to interview for this column, Ron Plies was toward the top. Even though he’d moved out of the PCR some years ago, he had served it in many capacities over the years and still maintained his connections with some of the members.

Unfortunately, my opportunity to do that was eliminated when Ron passed away on May 31. Instead of an interview, what I decided to do was share some of the insights and experiences I had from having known Ron since he first joined the NMRA in 1997. I’ve also contacted some other PCR members who worked with him or were his friend (usually both).

You can read about some of Ron’s background in the Last Run printed elsewhere in the *Branch Line*; but I will say that first and foremost, he was a man of principle. The primary areas of importance to him were his faith, his family, and the areas in which he could be of service (in both business and recreational activities). We in the PCR were fortunate to have been able to observe many of Ron’s activities and to see the impact he had on the organization.

The first time I became aware of Ron Plies was in 1997, shortly after he had become a NMRA member. Since he lived in Fortuna, he was by definition a member of the Redwood Empire Division; and because RED was scheduled to host the 1999 PCR Convention in Ukiah, the search for convention committee members had shifted into high gear. One of the things the convention chair was trying to do was to involve people from all geographic areas of the division; so when he saw that Ron had become a member, he contacted him and asked if he would be willing to work on the convention. (Pause for full disclosure: the convention chair for this event was Gus Campagna, which was one of the reasons I became aware of many things that were going on.)

Ron said he would be happy to, but he wasn’t sure how much he would be able to do from the far northwest part of the state; whereupon Gus, having given some thought to this, proposed that he take on the task of Clinics Chair. Ron had some concerns about how well he would be able to do the job from so far away, and about not knowing very many people in the division, let alone the region; but Gus pointed out that it would be an ideal way to meet lots of people and find out about their skills and interests. Since Ron was also a dedicated member of the Southern Pacific Historical & Technical Society, he soon found that there were other SPH&TS members in the PCR; and before long he was off and running with lining up clinics and clinicians for the convention. (I suspect his career as an insurance agent made him even more adept at marketing the opportunities.)

Once the 1999 Convention was over, Ron made a point of coming down for the RED meets whenever he could; and soon he and his wife, Carol, had expanded their circle of friends and acquaintances. He was enthusiastic about becoming more involved with RED and PCR; and his next opportunity came when the RED Director position came up for election in May of 2002. This time it didn’t take nearly as much effort to convince him that he would do a great job at it; and since Division Director is another one of the jobs that can be done from a remote location, with only two in-person meetings required per year (no such thing as Zoom back then!), he agreed to throw his hat in the ring. As a result, he spent the next four years of his NMRA career serving as the interface between the RED and the region, during which time he met even more people and gained even more friends/friendly acquaintances.

One of his first assignments as director was something that many people who were there will always remember. Since Ron had just been installed as RED Director at the PCR board meeting at the beginning of the convention, he was informed by the outgoing director that he would be presenting the Member of the Year award at the convention awards banquet.

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Looking Back, continued

He pleaded that since the outgoing director had selected the recipient, they should be the one to present the award; but his argument was benevolently ignored, and Ron spent a frantic hour getting as much information as he could about the recipient so as not to embarrass everyone involved. As it

turned out, his fears were unfounded: he came up with a clever way of introducing the recipient by asking their spouse to escort them up to the podium to receive the award; and there was a standing ovation for both the recipient and the presenter!

He also continued to be involved with RED activities as far as he was able; and for the RED summer meet of 2003, Ron organized an entire weekend of activities for the members and their families. He arranged for a block of rooms at a local Best Western in Fortuna (and arranged to use their conference room to stage a “movie night” for the people who arrived early on Friday; he arranged to have the Saturday evening banquet in Ferndale at an old-time restaurant with entertainment provided by local actors; and he organized a group breakfast out at the Samoa Cookhouse on Sunday morning before the attendees departed. In addition, there were layout tours on Saturday morning and afternoon, as well as a prototype equipment tour; and some folks even took in some more layouts as they traveled back home. It was a very enjoyable weekend, and it provided an excellent opportunity for many of the RED members to meet people they never had before (or hadn’t seen in person for a long time).

During the years he was RED Director, he always made a point of coming to the PCR Board meetings (or at least sending in a report). He was always willing to contribute to a discussion and offer suggestions; and while he clearly had his own point of view, he was respectful of other people’s opinions even if he disagreed with them. He continued to attend RED meetings whenever possible, often staying at Steve and Carol Skold’s house; and Steve would organize events like backyard barbecues in order to give Ron’s other RED friends a chance to visit with him. He was also named as the 2007 Member of the Year for the Redwood Empire Division as a recognition for all his efforts on behalf of the RED.



In the meantime, he continued to build models and enter contests, working to attain the necessary qualifications for his Master Model Railroader certificate. From the time he had joined the organization and learned about the Achievement Program, he had set a personal goal of earning his MMR; and his contest entries often won awards, some of them earning Merit Awards which would count toward his MMR. At the same time, he was learning new techniques from other modelers, which were also useful in his ongoing participation with the Eel River Valley Model Railroad Club.

In 2006, when he was termed out as RED Director, he looked forward to being a POM (Plain Old Member) and working even more intensively on his modeling, construction and operational skills. But fate had another plan! Early in 2007, he was approached and asked if he would be willing to run for PCR President. After picking his jaw up off the floor, he politely asked the questioners if they were serious. They assured him that after having worked with him in various capacities over the years, they were convinced he would be an excellent candidate for the position. Eventually they were able to convince him, and he was duly elected and installed in the top office.

Continued on page 38

Looking Back, continued

At the same time Ron was elected president, Pat LaTorres had been elected PCR Vice-President; and occasionally the two of them were referred to as “The Odd Couple”. Ron was older, traditional, from southern California, and extremely conservative both in politics and religion; Pat was

younger, innovative (a true child of the 60s and 70), from northern California – specifically, the Bay Area – and, like many of his cohort, was definitely on the liberal side of the equation. (The general speculation was that Ron was supposed to be the stabilizing influence that kept Pat from going off on tangents, whereas Pat was supposed to be the innovative influence that encouraged Ron to think of things outside his traditional comfort zone.)

As it turned out, Ron and Pat formed a remarkable and effective team. They might have accepted each other partly because of their shared interest in their mutually beloved Southern Pacific, but eventually acceptance morphed into a genuine respect and friendly affection for each other. Their cooperation influenced the board to work together in creative and different ways to address the region’s challenges (finances, membership decline, etc.).

When I was asking Pat for some of his thoughts about Ron, the main thing he mentioned was the fact that although Ron was extremely conservative, he was also welcoming and respectful toward people, even if there were aspects of their lives that conflicted with his view of the way things should be. Never did he allow any of his personal feelings or beliefs to get in the way of working with other people to help them do their jobs in the organization, to become better railroader or modelers, or just to get to know them better. (For the record, I agree completely with Pat’s observation, because I noticed and marveled at the same thing. And I concur with the “conservative” part: although I had a number of spirited political arguments with Ron, where we clearly held different viewpoints, they never included any personal attacks or slurs. After the end of each one, we were still as friendly with each other as we had been before.)

When Ron achieved his MMR status and received his award at the national convention, Pat came up with an ideal “congratulations, Mr. President!” gift which he presented to Ron on behalf of the PCR. Pat had long been an aficionado and collector of railroad china – especially the SP’s – and since he knew of Ron’s interest in the SP, he thought a piece of SP china would be a memorable and appropriate recognition of Ron’s accomplishment. He obtained a lovely cup and saucer set from one of the china patterns, which he presented to Ron after the official awards ceremony. Many of the PCR members who were at the convention were able to be there and watch as the VP gave the region’s gift to the President along with a “well-done” speech.



At the end of his term as PCR President, Ron expressed his thanks and encouragement for the incoming board in his president’s message from the **Branch Line** right before the elections for president and VP were held. To quote from his column: “This has been a good run for me being the engineer on the PCR Express and I trust it has been for you as well. The company rule book says I must swing off at this division point in May and let a new crew take over. At this writing the crew is yet to be called, but from those who are on the board it looks to be an outstanding group. I know whoever is called for this train will do a great job.”

Continued on page 39

Looking Back, continued

Once his term in office was completed, Ron and his wife Carol officially joined the legion of “retired” folks (you know, the people who are so busy they have no idea how they ever had time to work!) and started actively working on the process of selling their business, preparing their home for sale, and getting ready to move to Idaho to be closer to family (especially grandchildren). Although Ron would miss his PCR friends and colleagues, he was excitedly looking forward to the next chapter of his life, including becoming a member of the PNR and getting involved with the Caldwell, ID Model Railroad Club. Once they bought their new house in Idaho, one of the first things Ron did was to set up a train room in the garage where he could install the layout he had brought with him when they moved to Idaho. Between the PNR, the SPH&TS and the local club, Ron continued to and use all the skills he had acquired during his time in the hobby (and especially in pursuit of his MMR).

As time went on, Ron’s health began to fail; and between strokes and other illnesses, he eventually spent the majority of his time in a wheelchair. However, this did not deter him from pursuing his favorite hobby: Walt Schedler shared some of his memories of Ron with me, and one of them was about when Ron wanted to go to the 2018 SPH&TS convention, which was in Monterey. So Ron flew to Monterey, where Walt and his wife Carolyn picked him up and they went to the convention. Walt said that even though Ron spent the event in a wheelchair, he was “still all smiles”. This arrangement worked so successfully that when the in—person conventions resumed after the COVID pandemic, they repeated the process: for the 2024 convention, Ron flew into Sacramento, Walt and Carolyn picked him up and they headed east to Sparks, NV for another few days of SPH&TS fun and fellowship. (They had planned to do the same thing for the 2025 convention in Tucson; but, alas, the universe had other plans.)

Fortunately, although Ron had been in failing health, the actual end came relatively quickly. Steve Skold told us that Ron had sustained a severe fall several days before he died; and he peacefully slipped away, with his friends and family members in attendance, on May 31.

Ron Plies: husband, father, grandfather, pastor, model railroader, leader, friend, and – above all – a man who dedicated himself to his faith, his family, and every organization with which he was affiliated. He served the PCR well, and his influence has lasted far beyond his actual time in office. Was he perfect? Of course not – he was human. Was he someone who believed in doing the best job he could, and trying to make things better than they had been when he had become involved? Unquestionably. Though he’s shuffled off this mortal coil, those who knew him will remember him; and those who have only heard of him can have some idea of what he was like.

(Note: I want to thank all the people who shared their memories of Ron with me, as it gave me an even more comprehensive picture of who he was. In particular, I’m indebted to Pat LaTorres of the Coast Division, Walt Schedler of the Sierra Division, and Steve Skold of the Redwood Empire Division for their comments, especially since they each come from a different division in the PCR, which further indicates the extent of Ron’s friendship and influence. And in particular, I want to thank Carol Plies for the many conversations we had, both when they lived in California and when Gus and I visited them in Idaho. She gave me some special insights into Ron’s life and character, and gave me an even more comprehensive picture of him.)





The Daylight Limited convention has wrapped up—and what a great time it was! Nothing compares to gathering in person, sharing our passion for trains, and making memories together. From Bruce Morden's guided Amtrak tour over the breathtaking Cuesta Grade to visiting the Santa Maria Valley Railroad yard and seeing the exciting progress on their business car, our hosts made us feel right at home and gave us the opportunity to learn more about their railroad. Every part of the 4-day event offered something special. It was a joy to meet many first-time attendees and reconnect with familiar faces. Thank you to everyone who helped make it a success!

Our Division continues to thrive because of the energy and passion that each member brings—and that includes our newest members. They are the future of the Daylight Division. Their perspectives, skills, and enthusiasm are essential as we grow and evolve. Whether brand new to the hobby or returning after a break, we want everyone to feel welcomed, valued, and involved from day one.

To support our newest members, Dave Theis and Doug Wagner are leading the charge with a series of meetings and hands-on workshops—including a step-by-step layout-building series—designed especially for those just getting started. We want to help you build skills, confidence, and friendships through fun, engaging experiences. These programs are just the beginning, and we welcome your ideas on what would be most helpful.

Be sure to read Dave's message, "Stepping Out of our Comfort Zone" on the next page.

If you're a newer member, we want to hear from you! What topics, activities, or events interest you most? Your feedback will help us tailor future programs that are meaningful and useful to you. Please don't hesitate to reach out—your voice matters.

To our long-time members: remember how fun and encouraging it was to meet skilled modelers when you were just starting out? Now's a great time to return the favor. Reach out, strike up a conversation, offer a tip or two—it can make all the difference to someone who's just discovering the joy of this hobby. Helping others find their footing builds the kind of community we're all proud to be part of.




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Stepping out of our Comfort Zone

By Dave Theis, Daylight Division Member

As someone who has only been back in the hobby for a couple of years, I can honestly say that being a new guy in this hobby can sometimes be intimidating. I didn't understand this new way of operating called DCC (Digital Command Control) and it certainly seemed like everyone was speaking a new language and using terms that I was unfamiliar with.

Maybe some of you, who are new to the hobby as well, can relate to being in this position. If so, I would like to encourage you because, with just a small amount of effort, and by being willing to stretch yourself out of your comfort zone just a little bit, this situation can be overcome.

First, as a new person to the hobby there are going to be things you don't yet understand. DCC is different than just moving the dial on a transformer and watching a train move. And while that was fun and enjoyable, the hobby has become so much more than just running trains in never-ending four-foot circles. Modern layouts are now capable of mimicking realistic railroads with operating procedures and dispatching that allow you to have hours of fun running trains, making drop offs and pick ups at various industries, and interacting with multiple trains and users on the layout at the same time.

Second, it will most likely feel like people in this hobby are speaking a foreign language. For a short time it will be difficult to figure out what they are talking about. Don't let this discourage you. Think about it. No one learns a new language overnight, and the language of model railroading is no different. And here is the really good news: If you are not sure what they are talking about, just ask them. The seasoned veterans of our hobby, and model railroaders as a whole, are more than happy to help the new person and are willing to take the time to explain what they are talking about. Trust me, it won't happen immediately, but soon you will begin to pick up on a few things here and there. Before you know it, you will be right there speaking the same language that they are.

The key to your success really comes down to your willingness to learn and grow. Having been in your position, I made the choice early on to learn more by joining my local model railroad club, joining the NMRA and, becoming part of the Daylight Division of the Pacific Coast Region. Doing so gave me access to a place to run trains, opportunities to learn about modern model railroading, and access to a number of experts and people who truly took a vested interest in helping me to grow in the hobby. On top of that, and since I wanted to step out even further, I joined two NMRA Special Interest Groups (SIG's): The Operations SIG and the Layout Design SIG. Both groups were very welcoming and I have made a number of contacts and even made some new friends in the hobby.

Fast forward a couple of years and I have now been to the annual conference in Long Beach, a PCR Convention, a couple of LDSIG and OPSIG weekends, and I am part of a local club that is running trains and having fun. I am even building a Free-Mo Module that can be used for club layouts or Free-Mo events that take place all over the United States. All of which are things that I never thought or imagined I would be doing just a few short years ago.

And that, my friends, is the beauty of our hobby. Opportunities for growth and enjoyment are endless. The resources to help you learn are everywhere you look. And the people in this hobby truly want to see you find success and enjoyment it what many describe as "The Greatest Hobby in the World."

Remember, you don't have to start out knowing or even understanding everything. You just have to be willing to stretch yourself a little. And then, before you know it, you will get there.

I would also add this: Don't be afraid of getting a few bumps and bruises along the way. Sometimes we learn through our mistakes and, trust me, projects don't always come out the way you planned. Just the other night I spent three hours trying to connect a Digitrax[®] throttle to a module so it would run untethered from the layout, only to find out, when I finally asked for help from a Digitrax[®] forum, I had bought the wrong throttle. Well, no wonder it didn't work. Yes, it was frustrating, but I learned something new and, after all, isn't that what the journey is all about?

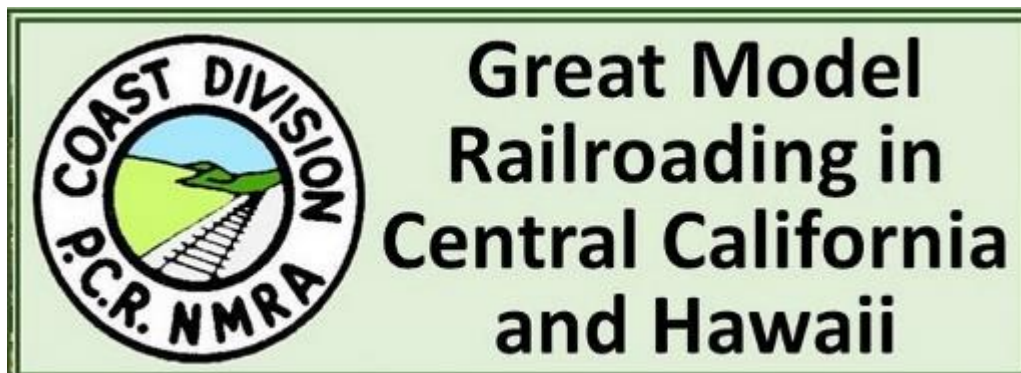
I hope this article has been an encouragement to your growth in the hobby. I also invite you to share your story and email me at Pastordave.jsaw@yahoo.com. I would love to hear about your progress and how you overcame some of the challenges that were present when you took your first steps into the world of model railroading.





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There are currently 109 recorded videos comprising nearly sixteen hours of model railroading content. Make sure you have plenty of time!!

Continued on page 44

Coast Division continued

By Phil Edholm

In Memoriam



Early 2025 has been a challenging time for great layouts in the Coast division. So far in 2025, we have lost three amazing layouts that have been stalwarts for both layout tours for convention and events as well as operating sessions. All three layouts will be long remembered in photos, articles, videos and the memories of all the modelers that visited through the years.

Longtime member and active layout operator David Parks passed, and his outstanding layout was removed. David's Cumerland West layout was an outstanding combination of great modeling, operating possibilities and automated operations. David and his crew hosted great operating sessions and introduced many to the world of large-scale prototype operations. A TSG Multimedia video of the Cumerland West layout can be found [HERE](#).



Ted Stephens, MMR made the difficult decision to close down his layout this year. Built in an industrial storefront, the Ohio & Little Kanawha Railroad featured outstanding modeling and great operations. Ted built the railroad while achieving his MMR and it also featured many structures by Earl Girbovan MMR. The layout was a staple on south bay layout tours and was visited by thousands through the years. The outstanding modeling and a great operating scheme were inspiration to many in the Coast and beyond. A TSG Multimedia video of Ted and the Ohio & Little Kanawha can be found [HERE](#).

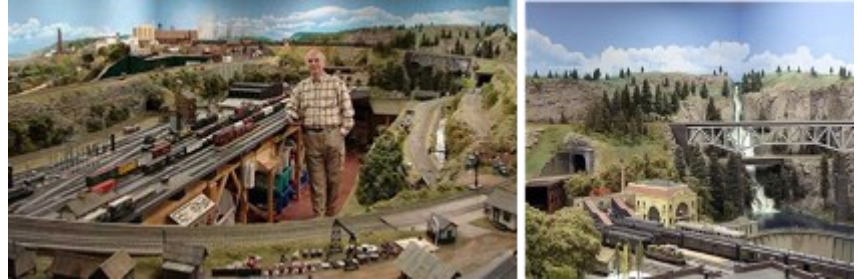


Continued on page 45

Coast Division continued

Ed Loizeaux has made the difficult decision to remove his New York Central -- Valley Division S scale layout. As Ed said,

this was not your father's American flyer layout, but rather an outstanding layout with over 300 feet of double-tracked mainline with a 6' tall waterfall, a cut stone viaduct over 10' long, and Mike Kotowski's professionally painted backdrop. The railroad was a great place to visit and an inspiration to creating a vision of a specific time and place. The immersive design and outstanding modeling and scenery left all who visited in awe. A TSG Multimedia video of Ed and his layout can be found [HERE](#).



Many thanks to David, Ted and Ed for building amazing layouts and being so generous throughout the years to share their work with all the modelers who visited and operated on their creations. Building and sharing layouts is a key part of our hobby and we appreciate all the effort that went into being available over the years. Thanks, your layouts will live forever in our memories.

The Coast Visit to the Hendy Iron Works – May 2025

On May 3rd, 2025, the Coast Division had a guided tour of the Iron Man Museum at what is now the Northrop Grumman Marine Division facility in Sunnyvale. This was originally the Hendy Iron Works, established after the original Hendy Works in San Francisco was burned down during the 1906 earthquake. Hendy started out during the gold rush building iron products for mining (hydraulic systems, ore carts, stamp presses, etc.) and evolved to a range of iron products. After the original facility in San Francisco was destroyed in the earthquake and fire of

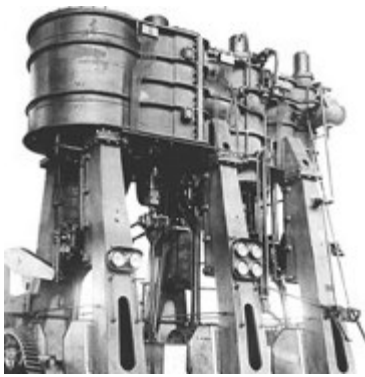
1906, Hendy moved to a large site in Santa Clara. In WW1 they produced some triple expansion marine steam engines and were the primary contractors for engines for West Coast built liberty ships in WW2. During WW2 they built over 700 engines for Liberty ships. After the war the facility

became part of Westinghouse Defense Division and was sold to Northrop Grumman in 1996. The museum was established in the early 1980s and, while small, has an amazing array of artifacts, models and products.



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Coast Division continued



We had about 30 Coasters come for the tour. Visiting the museum started with a presentation about the Hendy Iron Works



history and the current products and projects that are built on the site. The current facility has over 2,000 employees building various systems, primarily for the US Navy. This includes turbines for main engines and electrical power, gearing and transmissions for propulsion systems, as well as underwater missile launch tubes.

The museum has samples, models, and engineering drawings of a wide range of products built through the years. For example, Hendy produced many of the original iron streetlights in the Bay Area, including the ones in Chinatown with their signature pagoda designs. The ability of the US Navy to launch missiles from underwater was invented and developed at the site. The Navy has conducted over 700 launches using this technology without failure. This construction uses the unique skills of the site for highly accurate machining of a diverse set of materials. For example, many of the water exposed parts are made of very high nickel content stainless steel that is very hard to machine but loved by the Navy for being impervious to seawater corrosion.

The museum also houses a wide range of artifacts and images for the site over the years. Visits by US presidents and dignitaries are combined with images of generations of workers building complex mechanical assemblies in Sunnyvale. The museum shows how women entered the workforce during WW2 and were critical to the product efforts. While the site is not currently rail served, the CalTrain tracks are right across the street from the main entrance and the station is a short distance away. They still use rail inside the plant to move certain assemblies.

Overall, this was a great tour for the Coast and one enjoyed by all who attended. It was a great opportunity to learn about manufacturing, both in the past and in the present, all in the middle of Silicon Valley.

Coast Auction – June 2025

The Coast Division continued the tradition of having an auction for members on June 1st of 2025. The event was held in the Mastick Senior Center in Alameda. We had about 45 attendees to the auction. Phil Edholm did a clinic in the morning before the auction with about 10 attendees. The clinic was how he built the model of the Agnew Santa Clara depot that was at the 2025 PCR convention. After a brief business meeting, we proceeded with the auction.



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Coast Division continued

Unfortunately, the combination of Covid, eBay sales, and other factors have had a major impact on the number and overall value of items that were available for purchase at the auction. We



had less than 250 items available and many were no sale. While there were some great items, many higher value items like books did not sell. As a result, the overall auction was significant financial loss to the Coast Division. We are planning to have another auction in December. Hopefully that will have a significantly larger number of items and overall revenue. Another issue that emerged after the auction is banks being unwilling to cash checks made out to "Cash".



Traditionally, the auction has used buyers check made out to cash as a way to pay sellers. After the auction we had two major banks refuse to cash checks not made out to an individual (Wells Fargo and Chase). We are trying to understand the parameters of this for the next auction. Look for information about the next auction in October.

Train Shows in the Coast Division

We have generally had two Great Train Shows in the Coast Division through the years, at the Santa Clara (San Jose) and Alameda (Pleasanton) Fairgrounds. In 2024, the San Jose (February) show was cancelled. This seemed to be a harbinger of things to come as this show has been moved to the Solano Fairgrounds in Vallejo. This is unfortunate as these events are critical in promoting the hobby and meeting prospective new NMRA members. The lack of a formal train show in the South Bay area (population over 1.5M) is a major issue.

Future Events

We are planning our South Bay event at the South Bay Railroad Historical Society in September; dates will be announced soon. Look for announcements of other events in the Cost Extra emails and the PCR calendar.



Redwood Empire Division Report

From RED Callboard
Scott Lockhart, Editor



Twenty-five RED members attended the Spring 2025 Redwood Empire Division meeting.

Dave Croshere opened the meeting with comments about Ron Kaiser's status and thoughts on changes to meeting presentations before turning the meeting over to our new superintendent, Doug Smith. Members introduced themselves, followed by questions about notifying members about layout work sessions, how to communicate with each other, etc. Doug presented Dave Grundman with his RED Member of the Year award. The upcoming RED Summer Picnic was mentioned, with more information coming later. Dave Croshere suggested that meeting presentations be moved to the meeting room. The windows would be blocked with cardboard to make the room dark enough for presentations. There were seven Show & Tell presentations, which are described on the following pages. Wendy Moore brought a selection of model railroad buildings and equipment that belonged to Dudley Moore, a RED member who passed away last year. The models were offered for sale or given away to RED members. Lunch was held, show-and-tell, door prizes, and raffle winners were announced, and the meeting closed.



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Redwood Empire. continued



Dave Croshere (right) is presented with the 2025 RED Member of the Year Award by the new Superintendent/Director, Doug Smith

Congratulations to Dave Grundman, our Redwood Empire Division Member of the Year! Dave has been an active member of the RED as Chief Clerk and Paymaster, overseeing the meeting raffles and table of model railroad items that he hauls to and from meetings for door prizes, etc.



Dave received his first train set while living in Alaska (Army brat and proud of it), a Lionel 027 with light and smoke. Moving to the Washington, DC area, his dad gave him a Gilbert Northern Pacific passenger train set in HO scale. He still has that set. That got him started in real model railroading. Added to that was the Boy Scout magazine (Boys Life) series with monthly plans showing you how to build several structures and a 4x8 layout.

He found that passenger trains on a small layout can soon get boring, and then discovered how freight trains worked... much more interesting. Over the years and several small lay-outs, he

learned a lot about scale model railroading, and when asked by my fiancé, "Why do you do this?". My answer was "building my own world" along with the social aspects. As some of you know, Dave has a 12x18 room for his Great Northern layout, which sort of runs now but needs more buildings and scenery.

Show & Tell

Joan Fleck - John Huckaby's Models



"When I first joined CVL in 1996 with my son, I met John at the SRJC Day Under the Oaks. CVL was an annual display there as well as John bringing his portable layout down from Humboldt County. John's layout was very busy and full of humor. What I noticed right away was the red light district of the town, and the cult that existed at the time up in Oregon. It was hysterical!!! I thought to myself, this guy is really clever and funny. "

We would see him every year at Day Under the Oaks. More recently, he and his wife moved to Santa Rosa because of their health and mobility issues, living with their daughter, Donna. He found CVL and joined. That was our lucky day!!

I am grateful to have known him. He loved model railroading. He loved making people smile."



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"I presented my show and tell about the Maroon N scale train that I bought at the Kyoto railway museum. The Maroon train is a model of the private company train that runs in the urban area. I also have an associated book on the Maroon train. The train is four commuter style cars with one powered unit that also has pantographs.

I also brought an unpowered souvenir Shinkansen unit. This was also in N scale.

Generally, the Japanese love their train system, which is mostly passenger rail. Definitely not like most of the rail systems in the US."

Richard Mossholder



Rich brought a structure called VIGLICO (Very Important Generic Lineside Industry Company). This was built during the pandemic as a build-and-send with a friend Peter Loughlin who moved to Idaho. The idea was we would each build a section of the model and mail it back and forth without knowing what the other had done. Every time the package would arrive it would be a surprise and a challenge to add something creative. VIGLICO was the result of two people, 800 miles apart, sharing a project through a pandemic.

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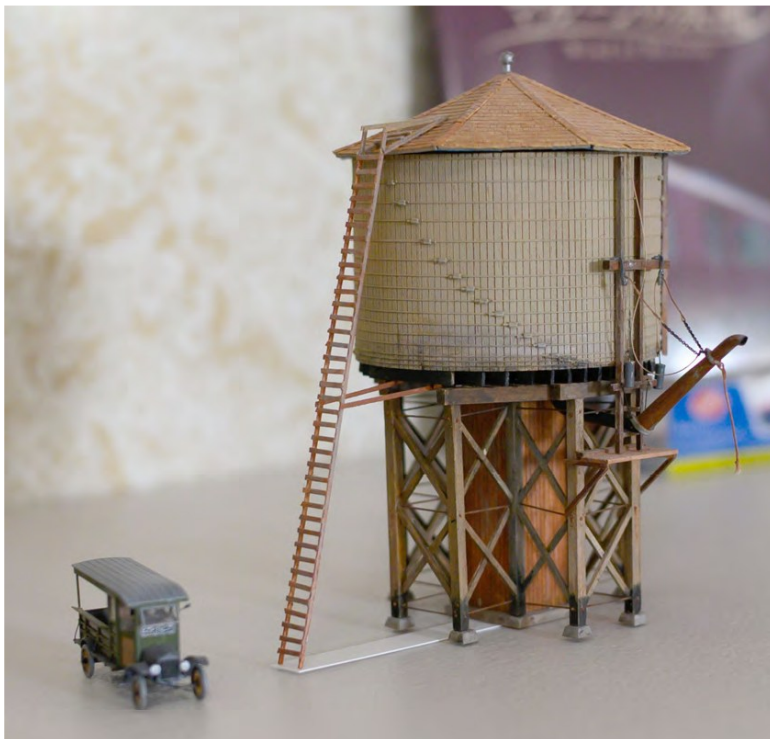


Giuseppe Aymar - Union Pacific Tank Car



UP training car for Hazardous Materials. UP would take it to different municipalities to train fire fighters and first responders in case of a Haz Mat accident. It had all different valves found in typical tank cars. TRANSCAER (Transportation Community Awareness and Emergency Response) events are multi-day, multi-modal training for multiple fire departments from the same geographic area that focuses on assisting communities to prepare for and respond to a possible hazardous material transportation incident. (www.transcaer.com) The model features multiple open doors and hatches.

Dave Croshere - Campbell Water Tower Kit



"My Show and Tell was a Campbell Water Tower kit. I particularly enjoyed the look of the heavy timbers the water tower sits on. I added some Tichy nut and washers to the truss rods. I altered the down spout to a more prototype appearance per some pictures and the counter weights were added. This water tower has sat on my layout for a couple of years and after a closer consideration I felt it needed more weathering."

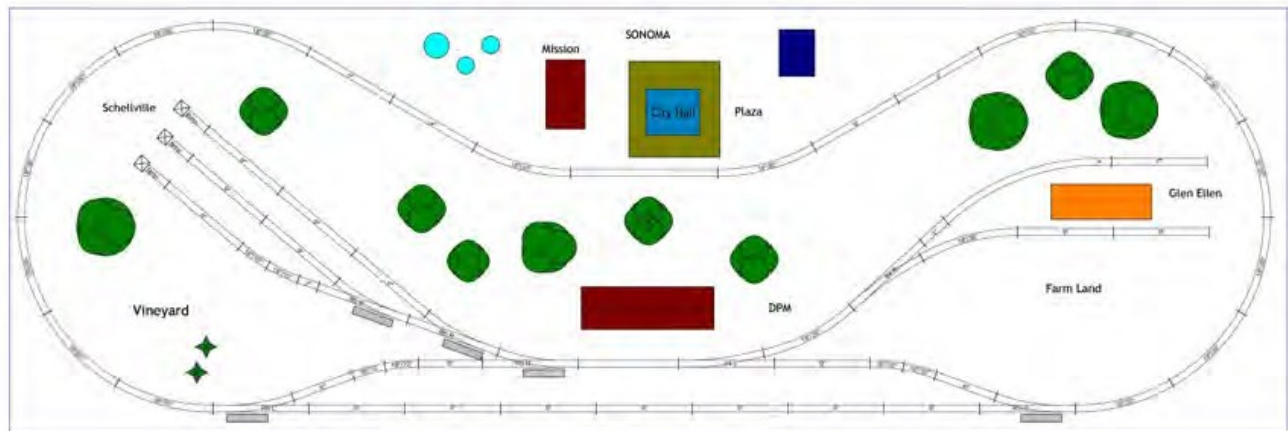
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Steve Lewis - New Layout at the Depot Park Museum



Members of Coastal Valley Lines, a NMRA 100% Club, are supporting Steve Lewis and Tom Curran in building a public HO display to be installed at the Depot Park Museum in Sonoma. The layout under construction is a 40" x 10' table top with an 18" radius curve, simple dog-bone design. It will include static track displays representing Schellville and Glen Ellen Station. DCC and DC capabilities are planned with an enclosure designed by Tap Plastic. Scratch buildings include Sonoma Depot by David Ulmer, Glen Ellen Station constructed by the late Dudley Peters (1939-2021), and whimsical buildings by John Huckaby (1936-2025). Help with building the Sonoma City Hall and the Sonoma Mission is needed .



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Ed Merrin - Photos of Gus Campagna's Layout



Ed presented photos he has taken of Gus Campagna's California Western Pacific layout using his focus stacking technique to enhance images from Gus' layout.

BEFORE



AFTER



Sierra Division Report



Morada Belt

The Sierra Division got a little off schedule earlier this year. The winter meet, that normally takes place the second weekend of February, had to be postponed until April 12. So, the meet that would have been scheduled for May was scrubbed.

The next Sierra Division meet will be August 23 in San Joaquin County, home of the **Tidewater Southern Model Railroad Club** and Dave Stanley's renowned **Moraga Belt Railroad**. What's more, pizza and drinks will be provided by the division. You know....a free lunch!

All PCR members are welcome to join Sierra for this field trip. Just contact Sierra Division Superintendent, Dave Putnam, at

sierra.superintendent@pcrnmra.net

or PCR President, Chip Meriam at

president@pcrnmra.net



14 JUL, MON

- NMRA Convention 2025 Sheraton Detroit Novi Hotel, 21111 Haggerty Rd, Novi, MI 48375, USA

15 JUL, TUE

- NMRA Convention 2025 Sheraton Detroit Novi Hotel, 21111 Haggerty Rd, Novi, MI 48375, USA

16 JUL, WED

- NMRA Convention 2025 Sheraton Detroit Novi Hotel, 21111 Haggerty Rd, Novi, MI 48375, USA

17 JUL, THU

- NMRA Convention 2025 Sheraton Detroit Novi Hotel, 21111 Haggerty Rd, Novi, MI 48375, USA

18 JUL, FRI

- NMRA Convention 2025 Sheraton Detroit Novi Hotel, 21111 Haggerty Rd, Novi, MI 48375, USA

19 JUL, SAT

- NMRA Convention 2025 Sheraton Detroit Novi Hotel, 21111 Haggerty Rd, Novi, MI 48375, USA

9 AUG, SAT

- Redding Model Train Show Win-River Resort & Casino, 2100 Redding Rancheria Rd, Redding, CA 960...

10 AUG, SUN

- Redding Model Train Show Win-River Resort & Casino, 2100 Redding Rancheria Rd, Redding, CA 960...

23 AUG, SAT

- Sierra Division at Tidewater Southern Club Fairgrounds, Stockton, CA, USA

Continued on page 56



EXTRA 2026 NORTH PCR CONVENTION APRIL 30 - MAY 3



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Convention HQ is the Sheraton Hotel
21111 Haggerty Road, Novi, MI 48375

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Monthly Activities

First Thursday - 10:00 AM / Siskiyou Model RR Club. / Currently meeting at member's homes. Info: Tom Brass, (530) 842-4921,

First Saturday 10:00 AM / San Leandro Historical Railway Society (HO, O, G), Monthly meeting / 1302 Orchard Ave, San Leandro, CA / Eugene 510-963-2985 / Info: info@slhrs.org.

First Saturday 11:00 am to 12:00 PM / Golden State Model Railroad Museum / East Bay Model Engineers Society (O, HO, N, Narrow Gauge, Traction) / Point Richmond, CA / Club Phone 510-234-4884 or info@gsmrm.org. Free coffee and doughnuts.

First Saturday 12:30 PM / European Train Enthusiasts (ETE) Sacramento Chapter (HO, HOm, N) 3600 J Street, Sacramento, CA. (entrance from 36th Street near East Sacramento YMCA). Social and H0 layout running 12:30 to 2:00 PM, then program/show and tell, followed by 2:30 PM business meeting. / Contact: Dusan Petras (650) 300-9504 or Jim Fischer (916) 965-7117, e-mail: dusanpetras@aol.com / See the website for additional information: www.ete.org.

Second Tuesday 7:30 PM / Empire Builders Model Railroad Club (HO) 3318 School St, Oakland, CA / e-mail: president@ebmrc.org

Second Wednesday 7:30 PM / Golden Empire Historical and Modeling Society (HO&N) Bakersfield, CA / Doug Wagner, e-mail: carldw@aol.com or 661-589-0391 / Club phone: 661-325-5820

Second Saturday 10:00 AM / Tehachapi Loop RR Club / Tehachapi, CA / Tom O'Brien, e-mail: threerail@earthlink.net 661-822-1546

Second (and Fourth) Saturday 10:00 AM to 3:00 PM (and every Wednesday 10:00 AM to 12:00 PM) • Admission: Free / Bay Area N-Trak Model Railroad Club (N) Wells Fargo Express Room, Southern Pacific Railroad Depot, 900 Loring Ave., Crockett, CA • Map / Donations are welcome. / Contact: Jim Gray, send text to 925-260-5804 or e-mail to gray-marks@hotmail.com

Second Saturday 1PM-4PM / West Bay Model Railroad Association, Open House, We run trains run for our visitors, 1090 Merrill Street, Menlo Park, CA / e-mail westbaylines.com

Second Saturday 1:00 to 5:00 PM / European Train Enthusiasts (ETE) San Francisco Bay Area Chapter (HO) Location varies / Contact: Brian Hitchcock, brianhitchcockdba@gmail.com / See the website for additional information: www.ete.org.

Second Saturday 1:30 PM / Carquinez Toy Train Operating Museum Business Meeting, 645 Loring Ave (Across from C&H Sugar), Crockett, CA. MAP

Second Saturday 3:00 - 6:00 PM / California Central Club (HO & HOm3) / Old Agnew Station, 4185 Bassett St, Santa Clara, CA / Subject to change, contacting us in advance of your visit is highly recommended at: ccmrc1961@gmail.com

Second & Fourth Weekends / Swanton Pacific Railroad Society (19") Santa Cruz County, CA / Ed Carnegie, e-mail: ecarnegi@calpoly.edu or 805-756-2378

Mid-Month Weekends / Bitter Creek Western RR (Live Steam) Arroyo Grande, CA / Dan Andrews, e-mail: danandrews@charter.net or 805-481-0419

Third Monday 7:00 PM / San Luis Obispo Model Railroad Association (all) Oceano Depot, CA / Dennis Pearson, e-mail: info@slomra.org or 805-929-3062

Third Tuesday 7:00 PM / Visalia Electric Model Railroad & Historical Society / Meets at Ryan's Place Restaurant, 3103 South Mooney Blvd, Visalia, CA / Cecil Eppler, e-mail: cecil_eppler@sbcglobal.net

Third Wednesday 8:00 PM / West Bay Model Railroad Association, Monthly Business Meeting, 1090 Merrill Street, Menlo Park, CA / e-mail westbaylines@gmail.com

Third Friday 7:00 - 11:00 PM / Black Diamond Lines Model Railroad Club Operating session (HO) / Antioch, CA / Info: info@blackdiamondlines.com / Club phone 925-779-1964

Third Saturday 9:00 AM - 12:00 PM Noon / San Joaquin Central Operating Session (HO) / Fresno, CA / Chuck Harmon, e-mail: harmonsta@aol.com or 559-299-4385. It's April 1949 on SP's Donner Pass and fictitious SJC. An advance courtesy contact is appreciated. Send e-mail to be added to mailing list.

Third Saturday / Sacramento Modular Railroaders Operating Session (HO) / Sacramento, CA / Scott McAllister, e-mail: scooter923@att.net

Fourth Wednesday 7:00 PM / Coast Toy Train Club / Wood Glen Hall, 3010 Foothill Rd (at Alamar), Santa Barbara, CA / Lots of collecting in S, O, and Standard gauges. / Contact: Ken Kelley, e-mail: kenatttr@thirdrail.com

Fourth (and Second) Saturday 10:00 AM to 3:00 PM (and every Wednesday 10:00 AM to 12:00 PM) • Admission: Free / Bay Area N-Trak Model Railroad Club (N) Wells Fargo Express Room, Southern Pacific Railroad Depot, 900 Loring Ave., Crockett, CA • Map / Donations are welcome. / Contact: Jim Gray, send text to 925-260-5804 or e-mail to gray-marks@hotmail.com



Enjoy the fellowship and learning experiences of a club near you

Alameda County Central Railroad Society

ACCRS is an HO and O scale club with layouts in a 30x100 building on the Alameda Co. fairgrounds in Pleasanton. With over 60,000 annual visitors, ACCRS is a great showcase for the hobby. Layouts reflect Alameda County and California and are continually improving and updating. Open for Fair events and Fridays, 6-9 PM. Lowest dues in the area.

Contact: Gary Lewis 925-455-8135

Email: glgslewis@comcast.net

Web: www.pleasantonmodelrr.org/index.html

Amador County Model Railroad Museum

The ACMRRM is located at 13828 Gold Mine Road, #1, Pine Grove, CA. Gold Mine Road intersects with Hwy 88 in Pine Grove. Our non-profit association is open to the public from 10 AM to 5 PM, Friday through Sunday during Winter months and Wednesday through Sunday during the Summer. Work sessions are held every Tuesday starting at 10 AM. Our scenic DCC HO-scale layout depicts the lumber and gold mining history of Amador County. Operations include the Amador Central short line as well as the railroads of California's Central Valley including terminals at Stockton and Sacramento.

Contact: Robert Piety, 209-296-3587 email rdpiety@sbcglobal.net

Antioch Model Railroad Club

The Black Diamond Lines is an HO Club located at 425 Fulton Shipyard Rd, Antioch. The BDL has been in continuous operation since 1981. Our Club is open to visitors on Tue. & Fri. from 5 to 7PM. We hold monthly Operations Sessions (4 hr.) on the LAST Saturday of each month, from 10 AM to 2 PM using Train Orders. We are a DCC (only) layout and we are kid friendly, although anyone under 18 has to have a related adult present, to run on the layout. For more information, info@blackdiamondlines.com or visit us on the web at www.blackdiamondlines.com and we can be found on Facebook and YouTube.

Bay Area Ntrak Model Railroad Club

BANtrak MRC invites N scale modelers to visit us at the Wells Fargo Express Room, Southern Pacific Railroad depot, 902 Loring Ave., Crockett, on Wed. and Sat., 10 AM to 3 PM. Contact: Bob Lewis, 925-283-6838
E-mail: BobLewis1@sbcglobal.net.

Bay Area S Scalers

BASS is a group of modeler railroaders in the San Francisco Bay Area focused on scale modeling in 1:64 scale. BASS is a round robin club, typically meeting in a member's home on the third Saturday of odd numbered months. Meetings may include a mini-clinic, or showing progress on models or a layout. We also have a modular layout that has been displayed at public events. Contact: Michael Eldridge (meldridge2000@gmail.com).
Web: www.bayareasscalers.org.

California Central Model RR Club

The CCMRC is located in the old Agnew depot (Santa Clara) of the Southern Pacific (4185 Bassett St.) directly across from the Lafayette St. gate of Agnew State Hospital. The theme of the 15' x 50' HO club is western railroading running from the Bay Area to Southern Oregon. We meet Fri., 7:30 to 11 PM. Operating sessions the 1st and last Fri. of the month. Contact: 408-988-4449 (operating nights), or Wayne Cohen, 408- 779-0707.

Carquinez Model Railroad Society

CMRS models the line from Oakland to Sparks with a double track mainline and many well-known lineside industries on three levels. The layout is in the 36 x 60 foot main room of the recently renovated former Oddfellows building that dates back to the 1890's. New HOn3 branchline coming soon. Entire railroad is NCE DCC-based with JMRI WiFi throttle support. We are open to the public bi-monthly with weekend open houses. Please see our web site (www.cmrstrainclub.org) and our listings in the PCR Master Calendar for more information. The club is open for perspective members and visitors during our Wednesday night work nights, 5PM to 9PM. We are located on the second floor at 645 Loring Ave., Crockett, CA 94525. Contact: Ron Lehmer, president@cmrstrainclub.org or Steve Huffstetter, secretary@cmrstrainclub.org. Club

number
510-722-3025



Continued on page 59

Coastal Valley Lines Model Railroad Club of Sonoma County, LTD

Coastal Valley Lines Model Railroad Club of Sonoma County, LTD (Coastal Valley Lines) is an association of novice-to-expert model railroaders from the Sonoma County area, who operate a digitally command controlled (DCC) HO scale modular railroad at public and private events, and gather for clinics, work sessions and monthly meetings. Coastal Valley Lines is incorporated in the State of California with its primary purpose of educating the public about model railroad-ing as a family hobby and is a not-for-profit 501(c) 3 organization with elected officers and bylaws. Additional information can be found at cvlrr.com.

Eel River Valley Model Railroaders

The Eel River Valley is an HO club with a NWP-layout series of modules under construction. We meet Fri., 7:30 PM at the Humboldt County Fair Grounds Commercial Building, NW corner, Ferndale. Contact: Jack Jensen, 707-845-2823, 3313 Union St., Eureka, CA 95503 or Ron Plies, 208-229-7107, 12818 N. 11th Ave., Boise, ID 83714.

Elsie The Left Coast (Elsie) HOn30 group invites anyone interested to join us. We use existing standards for module construction but are not merely a modular group. Contact: Al Sandrini in Bakersfield, 661-664-8614 or Ken Lunders in Cupertino, 408-777-9572.

European Train Enthusiasts

ETE (www.ete.org) serves modelers, railway fans, related organizations, and the community through public venues, operating layouts, the annual convention EuroWest, the chapter newsletter, and the quarterly national journal, the ETE Express. ETE is open to all who have an interest in European railways, whether full size or scale model. The SF Bay Area Chapter meets the second Saturday of the month from 1-5pm in various locations around the bay. Contact BillKeaney (650) 355-7231 or billkeaney@comcast.net.

Golden Empire Historical & Modeling Society

Bakersfield GEHAMS, founded in 1987 and in their current location since March '94, is dual-scale with a 30' x 100' HO, and a 18' x 80' N based on SP's route from Bakersfield to Mojave with Tehachapi Loop. Though under construction, mainlines are in with monthly operations. Contact: Doug Wagner, 661-589-0391 email: carldw@aol.com GEHAMS web: www.gehams.org

Golden State Model Railroad Museum – East Bay Model Engineers Society

900-A Dornan Drive in Miller-Knox Regional Park at Point Richmond. Weekly public hours are on our website. To meet members, inquire about membership and behind the scenes tours come 1st & 2nd Friday evenings of the month between 7:30 and 10 PM.; behind the scenes tours also available most Wednesdays 11 AM to 3 PM. New members are welcomed in all Scales – O, HO and N, including narrow gauge and traction. For general information check the website or call (510) 234-4884 (recording); for membership inquiries call 1st or 2nd Fridays 8-10p.m. (510) 236-1913. For other information or to schedule special events, Email info@gsmrm.org. Website: www.gsmrm.org

Humboldt Bay & Eureka Model Railroad Club

The HB&EMRC meets at their clubhouse and layout at 10 West 7th St., Suite #C in Eureka, on Sat., 7:00 PM. Visitors are always welcome. Business meetings 1st Sat of month, 7:30 PM. Contact: David Berriman P.O. Box 915, Arcata, CA 95518 707-825-7689

Monterey & Salinas Valley Railroad Museum

26 Station Place - Salinas, CA 93901
The Monterey and Salinas Valley Railroad Museum is dedicated to creating & maintaining a Railroad Museum in Salinas with an emphasis on the historic, contemporary and future value of the Railroad to the Salinas Valley and Monterey Bay Regions, providing experiential educational opportunities for children of all ages enhancing the core curriculum and inspiring civic involvement in our communities.

Open first weekends of each month 10AM - 4PM
831 789 8097

www.msvrr.org
info@msvrr.org

Napa Valley Model Railroad Historical Society

Located at The Napa Valley Expo, (fairgrounds), 575 Third St., Napa, our railroad runs from Napa to Ukiah with off line connections to Stockton and Portland, has 700 feet of main line, and large classification yards at each end. The layout is never finished, as we are always rebuilding and improving! Era is 1940 to present. Member-owned rolling stock stresses reliability and realism. We meet Fri. 7:30 PM to 12, with formal runs the 2nd Fri. of the month. Info: John Rodgers 707-226-2985 E-mail: Napa-John@napanet.net Web: www.nvmrc.org

Continued on page 60

Nn3 Alliance

An Internet-based fellowship of over 700 members in nine countries who model narrow gauge in "smaller scales". The term "Nn3" is used generally to describe 3', 3'6", and meter gauges, modeled in N or 2MM Scales. Members include local groups in the USA, UK and Europe whose portable exhibition layouts and modules regularly appear at conventions and shows. The "corporate" entity of the group - The Nn3 Alliance - created and published "The Nn3 Handbook", 140 pages with 400 illustrations, covering all aspects of small-scale narrow gauge, which remains the standard reference.

Web: www.Nn3.org
<https://groups.io/g/Nn3>

Sacramento Model Railroad Historical Society

Established in 1948, the SMRHS is located at 1990 Grand Ave., Sacramento. Modeled as the Sierra Central RR, both HO and HOn3 layouts. HO layout is being rebuilt to depict SP/WP prototype. Open Tues. and Fri. nights, 7:30 PM. Contact: 916-927-3618

Email: bod@smrhs.com
 Web: www.smrhs.com

Sacramento Modular Railroaders

Most of our events are operating sessions, where we make up trains in our large switch yard, and send road crews out to switch cars into and out of industries at the towns along the main line. The club owns a large main yard, the corner modules, some special modules, and some rolling stock. Members own their own modules and rolling stock. Contact: memberhip@sacmodularrailroad.com

Sacramento Valley Live Steam Railroad Museum (SVLSRM)

The SVLS was founded in 1968 and is located in Hagen Community Park, Rancho Cordova, CA. Our initial 1500 feet of track has expanded to over 6300 feet of mainline and sidings. Approximately 1500 feet is dual gauge 4 3/4" and 7 1/2". We can accommodate equipment ranging in size from 1 inch scale (standard gauge) to 5 inch scale (2 foot narrow gauge), the most common scale being 1 1/2". Public run days are on the 1st Saturday and 3rd Sunday of each month during our operating season

San Joaquin Valley Garden Railway Society

The SJVGRS was founded in July of 1995 to promote the joy of building and operating Garden Railways. We have grown to over 70 families in the Central Valley, meeting monthly at members' homes to spend the afternoon sharing our hobby, weather permitting. Dues: \$25 a year for a family. Contact info: Richard Emerson 559-439-7173
 E-mail: emerson.r@worldnet.att.net
 Web: <http://home.att.net/~sjvgrs/train/>

San Joaquin Valley Toy Train Operators

We are an all scale train club. We have members in scales from Z to G. Whether you are someone who likes to run toy trains in circles or model an exact date, railroad or scene or anything in between you are welcome. Everyone who likes trains is welcome. You don't need to own any trains. We have two portable layouts that we set-up at public locations. We have set them up at care facilities, Amtrak stations, train shows and other places. We also have outside club activities. We have gone on the train to Sacramento and the Sacramento Rail Museum. We've ridden the train at Railtown 1897 SHP and had a private tour there. Also we have visited home train layouts and will have many other activities in the future. Our meetings are at the Scouts Hall, 4717 Elm Street, Denair, CA at 1:30 PM (doors open at 12:30) on the first Sunday of each month. Holidays or other events can change that. We always get there by 1:00 PM to visit and see what others have brought to show. For more information please call Ed Cathcart at 209-479-4432

San Leandro Historical Railway Society

The SLHRS models the SP from Oakland/San Leandro to just east of Norden using the tri-level Donner Pass plan featured in the March '98 Model Railroader. It's housed in the former S.P., San Leandro depot, located at 1302 Orchard Ave., San Leandro (just off Davis St.). Work sessions Sat. 9:00 AM - 1:30 PM and Tues. 7:30 - 9:30 PM, business meetings the 1st Fri. at 7:30 PM. Contact: Eugene Brichacek, 510-963-2965
 email: embrichacek@gmail.com

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San Luis Obispo Model Railroad Association (SLOMRA)

The SLOMRA is a multi-scale modular group with active N and HO scale layouts. The goal of our nonprofit association is railroad education and local history through public display of our modules. Our shows usually include G-gauge, tinplate, and even LEGO trains. The monthly meetings include model and prototype activities, videos and discussions. The SLOMRA is open to new members who have a love of trains. The general meeting is the third Monday of every month at 7:00pm at the Oceano Depot. For more information visit www.slomra.org or email info@slomra.org.

Silicon Valley Lines

The SVL is located at 148 E. Virginia St., San Jose. We meet Fri. 7:30 to 11:30 PM, with business meetings the 1st Fri. and operating sessions the last Fri. of the month. Our HO layout utilizing DCC for realistic operations, computer-generated train orders and radio-based dispatching. E-mail: svl@siliconvalleylines.com Web: www.siliconvalleylines.com

Siskiyou Model Railroad Club

Currently we are meeting at a member's home on the first Thursday evening of the month. Contact Tom Brass as 530-643-1556. Currently a few members have been getting together on Tuesday mornings for various activities.

South Bay Historical Railroad Society

SBHRS invites interested modelers to visit us in the historic Santa Clara CalTrain Depot, 1005 Railroad Ave., Santa Clara, Tues. 5:00 to 8:00 PM or Sat. 10:00 AM to 3:00 PM. Contact: 408-243-3969. Web: www.sbhrrs.org.

South Coast Society of Model Engineers

The club consists of enthusiasts of all scales and prototypes living in the Santa Barbara area, and has a collection of railroad books and videos for loan to members. The club has no layout but activities include weekly operations and work sessions at member layouts, as well as occasional club field trips. We meet every 3rd Tues. at 7:30 PM, at Woodglen Hall, 3010 Foothill Road, Santa Barbara. Junior members welcome. Contact: Secretary David Kuehn, email: davidkuehn@cox.net. Facebook: <http://www.facebook.com/group.php?gid=163470062239>

The Tidewater Southern MRC Inc. & Tidewater Southern Railway Historical Society Inc.

We are Located in Building 1 at the San Joaquin County Fairgrounds (1658 S Airport Way, Stockton, CA 95206), We have both HO and N scale layouts housed in a 2,400 sq. ft. alcove. The HO scale layout represents the former Tidewater Southern branch line of Western Pacific. We meet every Saturdays from 9am - 5pm except for major holidays and run trains on the last Saturday of the month. Admission is free! Email us at: tidewatersrhs@gmail.com for our schedule.

Tri-city Society of Model Engineers

The TCSME located in the Niles Plaza is currently looking for new members to help build and operate both an N Scale and an HO layout focused on Fremont and surrounding areas. We meet Fridays 7:30-9:30 PM. The depot is located at 37592 Niles Blvd, Fremont, CA 94536. Please visit our web site at <http://www.nilesdepot.org/> <http://www.nilesdepot.org>

Walnut Creek Model Railroad Society The WCMRS located at 2751 Buena Vista Ave, Walnut Creek, is open on last Friday of the month from 8:00 to 10:00PM for operations. Fares are \$3.00 for 6-12 and seniors over 60 and \$5.00 for adults. Membership is always open to interested HO modelers. Contact: 925-937-1888 (recorder); Web: www.WCMRS.org. "Like Us" on Facebook (www.facebook.com/wcmrs). Please refer to www.wcmrs.org for the dates and times of our Annual, Weekend and Friday evening shows.

West Bay Model Railroad Association

The West Bay Model Railroad Association is accepting new members; all skills are invited. Construction and scenery are now complete on our DCC 2-level HO layout designed for operations; work on signaling is in process. Work sessions are Wednesday evenings and Saturday afternoons. Prospective members and visitors are always welcome. We run trains for visitors monthly on second Saturday afternoons. Business meetings are at 8 pm on the third Wednesday of the month, and operating sessions are on Saturday of the following week. We're located a few hundred feet south of the Menlo Park Caltrain station, in the former baggage building, at 1090 Merrill St., between the station and Ravenswood Ave. For more information, visit wbmrra.ning.com and facebook.com/wbmrra. Or e-mail westbaylines@gmail.com.



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chipmeriam@comcast.net

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Please update your contact information below.

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